



KENT/BRENNER/SPRINGFIELD REDEVELOPMENT PLAN

Central Planning Board: January 5, 2009

Adopted by Newark Municipal Council: April 1, 2009

City of Newark

Cory A. Booker, Mayor

Stefan Pryor, Deputy Mayor for Economic & Housing Development

Toni L. Griffin, Director of Community Development

Kent/Brenner/Springfield Redevelopment Plan

City of Newark, New Jersey
Cory A. Booker, Mayor



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A Redevelopment Plan submitted to the Central Planning Board on January 5, 2009 by the Department of Housing and Economic Development, Division of Planning and Community Development.

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Section 1. Plan and Neighborhood Overview

Kent/Brenner/Springfield at a Glance

The Kent/Brenner/Springfield Study Area comprises approximately 450 acres in both the Central and South Wards of the City of Newark, NJ, and is bounded by Bergen Street to the East, 15th Avenue to the North, Irvington to the West and Avon Avenue to the South. Encompassing most of Newark's "West Side" neighborhood, the Study Area is bisected by Springfield Avenue, which connects downtown Newark to surrounding municipalities and remains one of Essex County's most prominent arterials. The Study Area is further defined by two large open spaces, West Side Park and Woodland Cemetery. In recent years, the Study Area has experienced limited commercial development near its eastern boundary along Springfield Avenue.

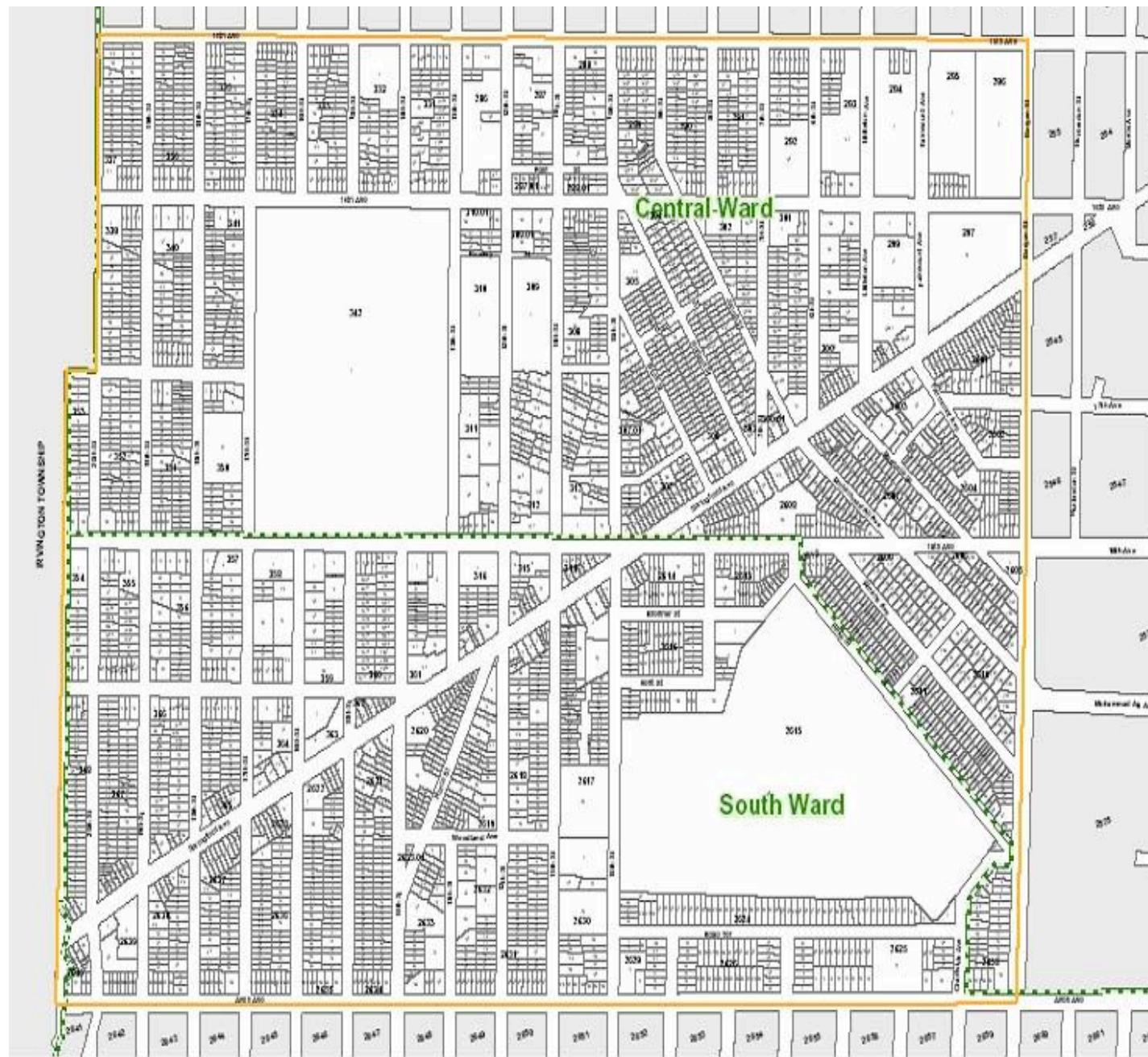
After a lengthy period of disinvestment, and following its location at the center of civil unrest in the 1960s, the KBS Study Area experienced a lengthy period of decline from which it is only now beginning to emerge. During this time, it suffered from an exodus of many of its residents and businesses, leaving behind abandoned and dilapidated structures, vacant lots, and creating other associated problems, including a lack of goods and services, public safety issues and a lack of affordable housing.

Compounding these problems, the Study Area was traumatized in 1999 when an airplane departing Newark Liberty International Airport crashed at the convergence of Kent Street, Brenner Street and Springfield Avenue. To commemorate that event, to honor the historical impact of this neighborhood and to reaffirm the belief that new hope is born out of the ashes of despair, this Plan is entitled the "Kent/Brenner/Springfield Avenue Redevelopment Plan" and endeavors to create a framework upon which the revitalization of this historic neighborhood can begin.

As noted, the Study Area has struggled in recent years to sustain itself as a vibrant commercial hub and residential neighborhood within the City of Newark. While the Study Area faces immediate challenges, it is important to recognize that this community also remains home to many assets that can be built upon as it pursues its reinvestment and community-building agenda. These include:

- Abundant open space, including West Side Park, a 30-acre public park located in the northwest part of the Study Area
- Springfield Avenue, a prominent regional thoroughfare that offers strategic investment opportunities
- 256 units of public housing currently existing or under construction, including the Hope VI development along
- Ample access to public transportation, including the planned introduction of two new terminals for New Jersey Transit's new pilot Bus Rapid Transit (BRT) program
- Multiple schools which have renovation and/or expansion plans, including a nationally recognized Blue Ribbon School in the Harriet Tubman School

- Recent reinvestment activity along the eastern portion of Springfield Avenue that can be used to anchor future development
- Strong interest from the private market in reinvesting in the Study Area
- Residents that are committed to revitalizing the neighborhood as a prominent economic and community center



Kent/Brenner/Springfield Redevelopment Area boundary and parcel map

The Planning Process

Some limited reinvestment has returned to the Study Area in recent decades, most notably in the form of the “big box” stand alone retail that characterizes suburban shopping centers. While providing some needed goods and services to the area, this development has done little to spur further revitalization within the KBS neighborhood. In response, City leadership and members of the KBS community determined that a comprehensive planning approach was needed and that a community-government partnership needed to be pursued.

To that end, the City of Newark, KBS community leaders, local community-based organizations, private market actors and KBS residents have collaborated to produce the Kent/Brenner/Springfield Redevelopment Plan. Throughout the planning process, community input was sought in multiple outreach sessions to insure that a collective vision for the Study Area was created. Along the way, suggestions for and modifications to this Redevelopment Plan were sought out and incorporated into the final document. In addition, Newwork, Inc., a Newark-based planning and design firm, was retained to provide design standards for the Plan. Rhodeside & Harwell, Inc., planning consultant for the Amelior Foundation, also lent valuable urban design expertise.

1.1 Statutory Basis

The purpose of the Kent/Brenner/Springfield Redevelopment Plan (“Redevelopment Plan” or “KBS Redevelopment Plan” or “Plan”) is to establish a comprehensive planning framework to guide future investment and redevelopment within the KBS Study Area. It is based on the City of Newark Municipal Council’s June 15, 2005 designation of the entire City as an “Area in Need of Rehabilitation,” pursuant to the New Jersey Local Housing and Redevelopment Law. (Newark Municipal Council Resolution #7RDO (AS) 061505.) N.J.S.A. 40A: 12A-1 et seq. This determination was based on a finding that 50% or more of the housing units in the City are 50 years old or older and that the water and sanitary sewer utilities are at least 50 years old and in need of maintenance and repair. This Plan has been developed to meet the requirements of Section 7 and Section 14 of the Local Redevelopment and Housing Law as well.

1.2 Applicability to Existing Planning Efforts and Relationship to Local Objectives

In completing this Plan, the 2004 Newark Master Plan, the South Ward and Central Ward Redevelopment Plans, the 2002 Springfield Avenue Smart Growth Handbook and the 2000 16th Avenue Business Survey were reviewed and re-evaluated. Where feasible, this Plan incorporates and advances the goals, recommendations and design standards of those documents. New zoning regulations and design guidelines were, however, incorporated into this Redevelopment Plan to guide future neighborhood-appropriate development and ensure that it is accomplished in a comprehensive and sustainable manner. Further, this Plan encompasses and supersedes the zoning in the current Springfield Bergen Redevelopment Plan, adopted in 2003 and amended in 2006. It does not supercede all other redevelopment powers pursuant to N.J.S.A. 40A:12A-1 et seq.. All further development in the Springfield Bergen Redevelopment Plan boundary must abide by the regulations in the KBS Redevelopment Plan.

The KBS Plan builds upon the principles laid out in the *Newark Draft Vision Plan* of October 2006, which was developed during a three-day charrette sponsored by the New Jersey Office of the Regional Plan Association and hosted by the New Jersey Institute of Technology at the

request of Mayor Cory A. Booker. Furthermore, this Plan is in accordance with the Newark City Master Plan and other neighborhood planning efforts, detailed below.

Newark Master Plan

A new Land Use Element of the *Newark Master Plan* was adopted on December 6, 2004, and contains several assumptions, goals, and policies which form the basis for the Future Land Use Plan. The KBS Redevelopment Plan is supportive of, and consistent with, the following assumptions, goals, and policies of that document:

- Provide a comprehensive framework within which community, neighborhood-based planning can be effectuated with grassroots community involvement
- Guide future development in a comprehensive and coordinated manner
- Rebuild neighborhoods and communities where there are large areas of vacant or marginally deteriorated land uses
- Utilize design standards and incentives that recapture the design character prevalent in the neighborhood
- Reinforce and capitalize on Newark's existing physical assets
- Revitalize and strengthen Newark's retail corridors along major arterial roads and concentrate new retail uses and renovation in nodes or centers at major intersections
- Accommodate both modern retail with off-street parking and traditional retail with convenient pedestrian and transit access with a community-oriented streetscape
- Encourage traditional mixed-use developments with ground floor retail and upper floor offices and apartments
- Provide opportunities for small convenience-oriented neighborhood retail centers within residential areas
- Identify the most appropriate adaptive reuses of former school buildings and sites to best serve the neighborhood
- Increase the opportunities for providing community services, particularly schools and day care facilities and services
- Provide opportunities for a wide variety of housing types and decent, safe, affordable housing
- Plan residential neighborhoods to allow for adequate open space and community facilities
- Encourage the creation of new parks and open spaces and the upkeep of existing open spaces

Ward Redevelopment Plans

The City of Newark has previously utilized the redevelopment planning process to improve areas that have been resistant to new development and improvement absent government intervention. The City has prepared Ward Redevelopment Plans for both the South Ward and Central Ward which focus solely on City-owned lots that have subsequently been declared Redevelopment Areas. These reports are guided by the land use element of the Newark Master Plan and envision the rebuilding of neighborhoods by providing increased housing and commercial opportunities, light industrial uses, community recreation, landscaped open spaces and landscaped parking areas. The KBS Plan incorporates and, where feasible, is consistent with the goals, permitted uses, and design standards of these two Ward Plans. This Plan builds on both the South and Central Ward Plans by focusing on all of the properties and land uses within the Redevelopment Plan Area, rather than just City-owned parcels, to accomplish a more comprehensive planning framework for the Study Area.

Springfield Bergen Redevelopment Plan

The 2002 Springfield Bergen Redevelopment Area is located within the eastern boundary of the KBS Redevelopment Plan Area and is bordered by Springfield Avenue, 18th Avenue, and Bergen Street. It includes tax blocks 2601, 2602, 2603, 2604, 2607 and 2608 and has been home to recent development activity, with a shopping center being constructed that contains a Home Depot, Wendy's and Applebee's. The Plan's overall goal is to create a retail/commercial use district, and the objectives of the Plan are to:

- Establish a land-use pattern that will enhance private sector economic development
- Concentrate business and commerce in the central corridor of the City where existing infrastructure and private and public investment are greatest
- Anchor contiguous business areas and economic clusters along the corridor

The Springfield Bergen Redevelopment Plan and the KBS Redevelopment Plan will complement and support one another in their respective revitalization goals within the Study Area. The KBS Redevelopment Plan will overlay the Springfield Bergen Redevelopment Plan's zoning. All other redevelopment powers of the Springfield Bergen Plan shall remain.

The West Side Park Neighborhood Plan

The West Side Park Neighborhood Plan is an un-adopted plan that incorporates all of Tax Block 342 and is bordered by 16th Avenue, 18th Avenue, 13th Street and 17th Street. The area surrounding the park is recognized in the Plan as spanning from 15th Street in the north to Springfield Avenue in the south, and from 9th Avenue in the east to 19th Avenue in the west. Specific areas of opportunity and techniques for overall renovation and revitalization, especially with respect to streetscape improvements, are identified in concepts developed in 2006 by Rhodeside & Harwell, Inc, with sponsorship from the Amelior Foundation. The proposed vision includes vibrant residential and commercial neighborhoods that are mutually supportive and linked by safe, convenient and attractive pedestrian access-ways.

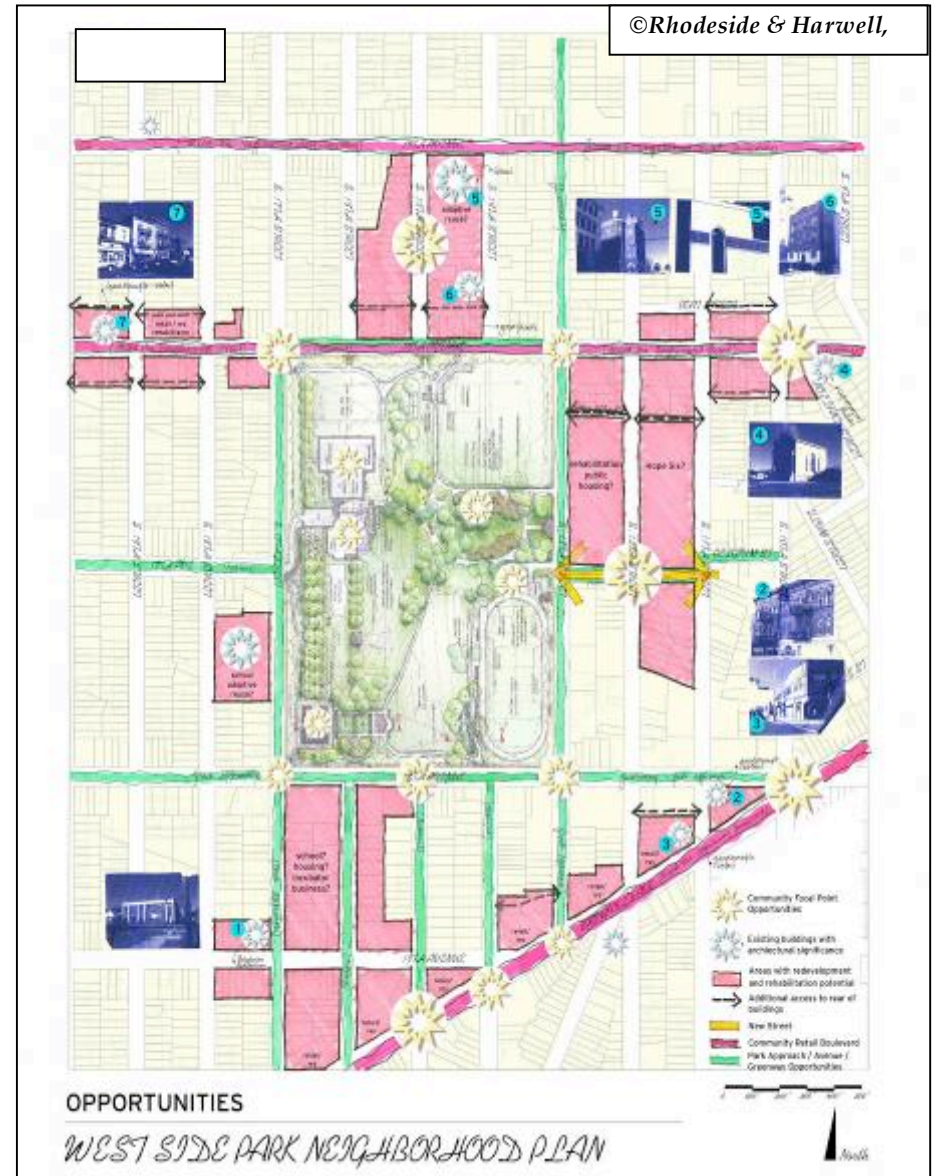
Land uses proposed by the West Side Park Neighborhood Plan correspond to those presented herein, and include:

- Concentrate business and commerce along pockets of Springfield Avenue where existing infrastructure and private and public investment are greatest
- Anchor contiguous business areas and economic clusters along Springfield Avenue and 16th Street
- Secure West Side Park as public open space and create additional “pocket park” areas in the vicinity
- Encourage mixed-use development in designated areas

Concentrating businesses along the entirety of Springfield Avenue will be contingent upon a significant population increase, as the resident population has been unable to sustain businesses on the entire span of Springfield Avenue in recent decades. The intersection of Springfield Avenue and 18th Street is a critical pocket where retail businesses have endured economic pressures, so 18th Street will also be recognized as a retail node, in addition to the 16th Street and Springfield Avenue commercial areas designated in the West Side Area Concept.

Design criteria proposed by the West Side Park Neighborhood Plan concepts are consistent with those presented herein and include:

- Pedestrian walkways and safe routes by way of designated crossing areas, street plantings, and traffic calming techniques such as concrete bump-outs, median pavers, and rotaries
- Architectural detailing to enhance existing and infill residential dwellings and commercial buildings
- Rear access to residential units by the creation of shared driveways, where possible, to reduce the sense of the garage as the most prominent feature and focal point of the dwelling



West Side Park concept plan as designed by Rhodeside & Harwell, Inc.

Based on the above analysis, the goals and objectives of the KBS Redevelopment Plan are substantially consistent with prior local objectives for the area and the 2004 Land Use Element of the Newark Master Plan. Further, the KBS Redevelopment Plan recognizes West Side Park as a neighborhood asset that offers development opportunities along its perimeter. Finally, this Plan recognizes preservation and adaptive reuse as tools for accomplishing revitalization of the Study Area and calls for streetscape improvements to the Study Area.

1.3 Existing Conditions: Challenges and Opportunities

This Redevelopment Plan seeks to revitalize the Kent/Brenner/Springfield Study Area through increased commercial capacity and by strengthening the neighborhood defined by Springfield Avenue. This will be accomplished by building upon existing community assets while simultaneously encouraging new investments in business creation and residential construction to reenergize the Study Area as a vibrant mixed-use neighborhood.

Those issues identified during community outreach sessions as most pressing within the Study Area, and therefore most emphasized in this Plan, include:

- Abandoned and derelict properties
- Lack of goods and services due to diminished commercial capacity
- Lack of affordable housing choices for members of the community
- Public safety issues, including crime, speeding, inappropriate curb cuts and a lack of parking

To realize this revitalization goal, it is necessary to first develop an accurate understanding of the existing conditions within the neighborhood so that appropriate redevelopment goals can be established and pursued. An analysis of local demographic data, zoning and land use patterns, economic activity and housing conditions was conducted to better inform the redevelopment effort and strategies to address community concerns.¹ What follows is a snapshot of a portion of the reviewed data and its applicability to the redevelopment planning process.

Study Area Demographics

¹ Demographic and economic data is based on census tract data retrieved from the 2000 census. While the census tracts relied upon closely approximate the boundary of the KBS Study Area, they are not an exact match, as seen in Appendix 3. The data provided in this section, therefore, represents an estimate of the KBS Study Area only.

- The population of the KBS Study Area totaled 11,515 in 2000, a sizable portion that represents just over 4% of the City's population
- Within the Study Area, the median household income was just over \$25,000, which is lower than the City-at-large and less than half of New Jersey's median household income of \$55,146. This indicates a need for affordable housing choices in the Study Area, including low-income and public-housing options.

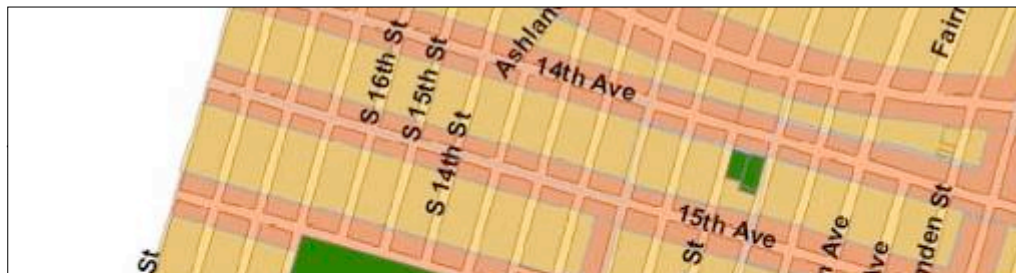
Study Area Population	11,515
Median Household Income	\$25,271
Residents of school age (17 and younger)	35.2 %
Families living below poverty line	51.4 %
Housing vacancy rate	14.2 %


- More than half of the households in the Study Area live below the poverty line, indicating a need for quality job creation as the City pursues its redevelopment strategy
- School aged children (17 years old and younger) comprise more than one third of the Study Area population (35.2%); a greater share than in the City of Newark (27.8%) and State of New Jersey (21.8%). Further, construction of new one-, two- and three-family homes has continued to attract families to the KBS neighborhood. This indicates a need to pay special attention to preserving and creating new open space and recreational opportunities within the KBS Study Area.

Zoning and Land Use



The Study Area already contains many diverse land uses, including residential, commercial, industrial, open space, and semi-public. There are also a number of institutional and community land uses, including several public schools and a number of places of worship located throughout the Study Area. Residential zoning throughout most of the area is currently R-3 residential, which permits one, two-, and three-family homes. There are small pockets within the Study Area that are still zoned Industrial I-1 as well that continue to house a few industrial buildings located between Springfield Avenue and Westside Park, as well as directly west of Woodland Cemetery.

Current zoning, however, is not reflective of the existing development pattern, necessitating the need for new zoning standards to guide redevelopment. For example, the numbered avenues running east and west throughout the Study Area are currently zoned for B-2 commercial uses, despite the fact that most are currently being used for predominantly residential purposes. A portion of Avon Avenue that is currently zoned as B-1 commercial is currently being used for residential purposes as well. Since the Study Area does not currently have the density necessary to support the level of commercial activity called for by the current zoning, the newly proposed standards will reflect the predominantly residential make-up of the Study Area. It will preserve strategic locations within the Study Area for the development of new commercial centers that will contribute to the creation of the vibrant neighborhood called for in the Redevelopment Plan Vision.



 B1 – Commercial

April 1, 2009

-  B2 – Commercial
-  I1 – Industrial
-  R2 – Residential
-  R3 – Residential

Housing

The residential neighborhoods within the Study Area are home to single and multiple-family houses and apartment buildings, as well as many vacant homes and lots, with an overall housing unit vacancy rate of 14.2%. Since 2000, the Study Area has experienced considerable in-fill residential construction, however, driven mostly by the non-profit community development corporations that are active in the area such as Macedonia Ministries, Corinthian Housing Development Corporation, CREST Community Development Corporation, New Communities Corporation and Tri-City Community Development Corporation.

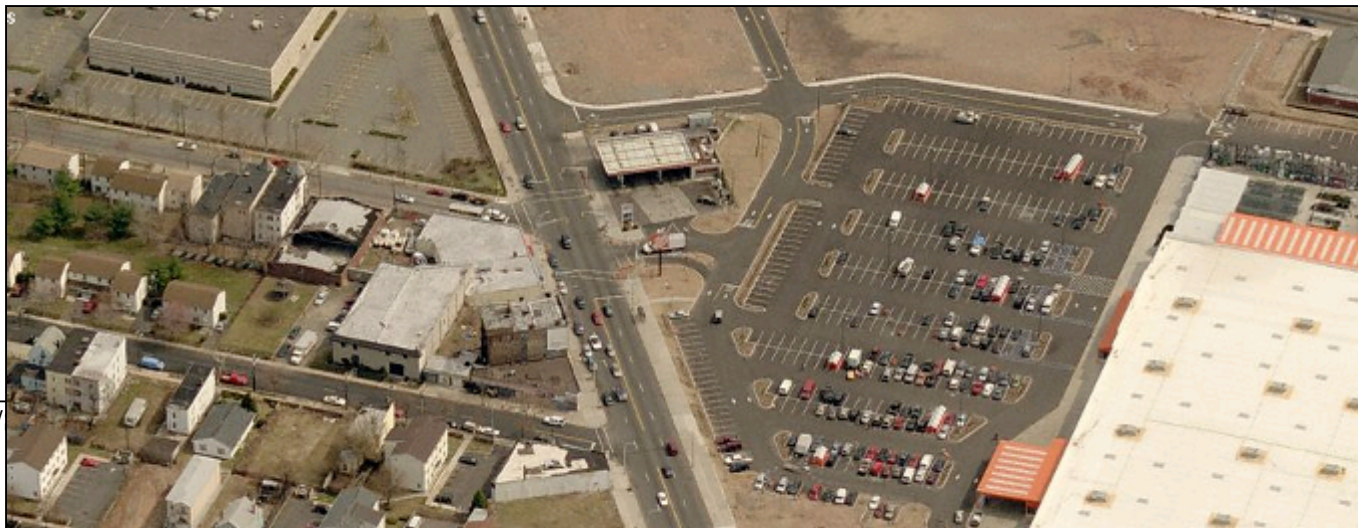
	Count	%
Total Properties	2633	100%
Non-Residential Properties	897	34%
Residential Properties (new and vacant)	1736	66%
Single family properties	497	29%
Multi-family properties	1116	64%
¹⁴ New construction	123	7%

According to a 2007 neighborhood survey conducted by The Reinvestment Fund (TRF), new residential construction now accounts for 7% of the Study Area's housing stock. Residential properties comprise two-thirds of the KBS Study Area, with multi-family and single-family properties comprising 64% and 29% of that share, respectively. This trend in residential construction highlights the need for future development to follow well-defined design standards to insure that new construction remains appropriate for the neighborhood and is accomplished in a sensible manner. Further, this Plan encourages the construction of affordable housing units, including public, low-income housing, and workforce housing for all members of the community – present and future – so that all Newarkers can afford to live in the Study Area.

Commercial and Other Uses

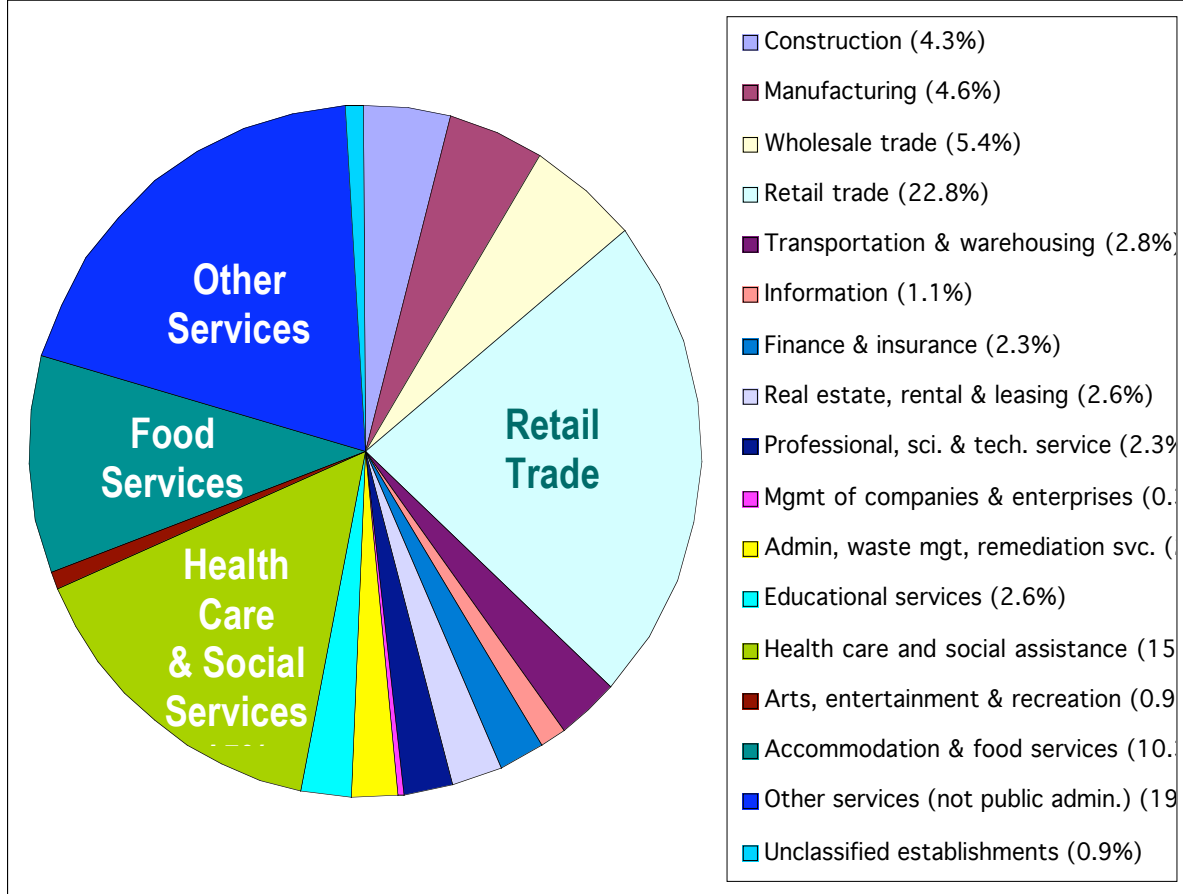
The goal is to re-establish Springfield Avenue as a thriving commercial strip within the City of Newark by attracting a variety of retailers and service providers that will serve local shopping needs while attracting customers from the surrounding area as well. Further, the introduction of new retailers is intended to provide new jobs for neighborhood residents to replace industrial employers who have since left the neighborhood and are unlikely to return given current market conditions. Springfield Avenue, which connects downtown Newark with surrounding municipalities, was historically a thriving retail strip lying along local mass transit lines. Now, however, it is host to only a small sampling of businesses which, while satisfying a narrow neighborhood need, fail to attract passersby and spur further economic development. Many properties that were once devoted to retail are now utilized as other uses which disrupt the retail street façade and discourage future investment in the area.

The eastern portion of Springfield Avenue has, however, seen some new investment in recent years in the suburban “big box” model, including the construction of a movie theater and Home Depot. A strategy of this Plan will be to build upon this recent development while remaining committed to creating a walkable, pedestrian-oriented neighborhood. To that end, revised zoning regulations will concentrate new commerce in strategic locations that take advantage of existing businesses while helping to create a thriving mixed-use neighborhood. Helping achieve this goal will be the planned introduction of two Bus Rapid Transit (BRT) stops within the area along Springfield Avenue, which offers a unique opportunity to capitalize upon the existing transportation strengths of the area to attract shoppers to the newly revived commercial district while providing residents with increased mobility to other parts of the region.



Birds eye view of the Home Depot site along the eastern stretch of Springfield Avenue

Within the remaining KBS business community, retail trade represents the largest segment of the commercial market. Health care and social assistance, accommodation and food services, and other service-based businesses represent significant portions of the local market as well, combining to represent more than 45% of the remaining market. Since residents have continually expressed their desire for basic goods and services within the Study Area, however, the broader revitalization goal is to broaden the total offering of goods and services along Springfield Avenue to ensure that residents are able to meet their needs locally while reestablishing the Study Area as a destination for all shoppers. Since grocers, pharmacies, banks, home improvement stores, professional service providers, family restaurants and other neighborhood-oriented commercial uses are those identified by residents as most lacking, they are the types of businesses that will be encouraged in this Plan.



Existing business make-up within KBS Study Area

Vacant and Abandoned Properties

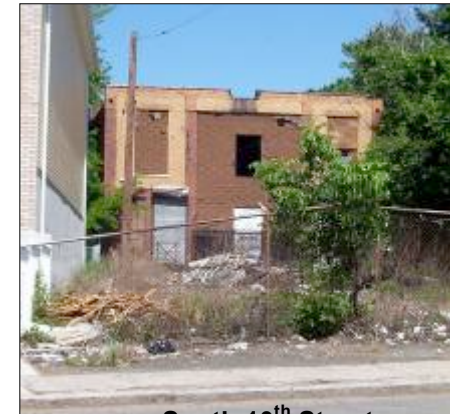
Despite the recent residential construction and limited redevelopment in the Study Area, there remain numerous blighting influences that pose a public safety hazard and act as a barrier to the comprehensive redevelopment of the Kent/Brenner/Springfield area. These include vacant and derelict homes, buildings and properties. To combat these negative influences, the City identified three “Areas of Concern” within the neighborhood in 2004, and notices were then sent to property owners within these areas addressing code enforcement issues, necessary clean-up actions or notices of rehabilitation/demolition issued by the City Police Department.



17th Street



11th Street



South 10th Street

Examples of vacant properties within KBS Study Area

Section 2. The Vision

The appropriate starting point for any redevelopment plan is to capture the community's vision for its future and to establish a consensus on what goals are to be achieved through implementation of the vision. The KBS Redevelopment Plan Vision is the result of several interactive workshops with community stakeholders to determine what they envisioned for their future. Succinctly stated, members of the community seek: "A safe, walkable neighborhood which provides neighborhood services, affordable housing choices for all, employment opportunities and a sense of community."

The following goals and objectives provide additional focus to the community vision that will guide and inform specific strategies aimed at achieving it, while the design standards found in Section 4 provide a framework for the physical redevelopment of the Study Area.

2.1 Goals and Objectives

Consistent with the community's vision for the KBS Study Area, the broad goal of this plan is to create a vibrant, commercially active mixed-use neighborhood within the KBS Redevelopment Plan Area. This will be done by creating new investment opportunities for private and public-private development by providing for a range of quality commercial, residential, industrial, and recreational uses that capitalize on the area's strategic location and assets and create jobs for neighborhood residents.

This Redevelopment Plan is based in part on the following planning principles from the New Jersey Office of Smart Growth:

- Revitalizing commercial districts
- Revitalizing neighborhoods
- Providing a mixture of land uses
- Providing a pedestrian oriented environment
- Capitalizing on mass transit opportunities
- Providing open space
- Providing a variety of housing opportunities
- Preserving natural resources
- Utilizing high quality design standards

Informed by the principles above, the following **goals** of this Plan detail where the KBS community would like to be while the accompanying **objectives** lay out the strategies that will be pursued to achieve the stated goals. They are as follows:

Goals

The long-term goals for the KBS Redevelopment Plan include:

- Revitalize Springfield Avenue through mixed-use development
- Create housing choices for residents of all income levels
- Ensure adequate and safe open spaces and recreation areas
- Ensure safety on neighborhood streets
- Create walkable, pedestrian-friendly streets that are home to commercial and retail uses while retaining a distinctive residential feel
- Improve overall neighborhood circulation and connectivity
- Recognize West Side Park as a neighborhood asset and capitalize upon it
- Discourage and eliminate uses incompatible with residential development
- Stabilize smaller neighborhood convenience commercial nodes
- Increase the sense of community and neighborhood pride
- Enhance the availability of local goods and services while increasing local employment opportunities by encouraging local hiring practices

Objectives

To achieve these goals, the objectives of this plan are to:

- Provide opportunities to expand and strengthen local and regional retail and commercial uses by implementing zoning that maximizes development opportunities along Springfield Avenue
- Facilitate the assemblage of land parcels of sufficient size and dimension to enable arrangement of new land uses
- Ensure appropriate rehabilitation of existing residential and commercial buildings, community facilities, and open spaces as well as new infill development through design standards
- Ensure that city owned land is developed for preferred neighborhood uses
- Create land use and building requirements specific to the Redevelopment Plan Area that are sensitive to adjoining uses
- Undertake infrastructure improvements including streets, curbs, sidewalks, parking, and open space

Section 3. Strategies for Revitalization

The ultimate revitalization goal is to reestablish Springfield Avenue as a thriving mixed-use corridor surrounded by a stable residential neighborhood. New investments in housing, business, open space and improvements to the physical environment are envisioned to address the concerns raised by members of the community. To accomplish this goal, specific strategies have been developed which focus on two

development patterns: the commercial/neighborhood center and the residential/mixed-use infill areas. Further, customized design guidelines will insure that development occurs in a sensible manner while addressing many public safety issues. It is important to emphasize that no residents will be displaced with this Redevelopment Plan. To the contrary, this Plan is intended to improve the overall quality of life of existing residents while drawing new residents to the KBS area.

Since Springfield Avenue is the main commercial corridor within this Study Area, it is appropriate for larger and more intensive commercial and residential development than the remainder of the neighborhood. This more intense development will be able to accommodate and sustain the planned Bus Rapid Transit (BRT) that will run along Springfield Avenue to Newark's Central Business District, consistent with the recommendations of Newark's October 2006 Draft Vision Plan. The planned BRT system, with two stops in the KBS Study Area, offers an opportunity for the community to capitalize upon the introduction of high-speed public transit to better link current residents to the City while providing easy access for the region to a newly revitalized Springfield Avenue. To insure that future development occurs in a manner that supports and sustains the BRT system, all new development along Springfield Avenue is required to coordinate with the City of Newark's Department of Engineering, Division of Traffic and Signals and New Jersey Transit.



A "Go Bus" which will run along Springfield Avenue as part of the planned BRT system.

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The larger commercial and regional shopping areas will be concentrated along Springfield Avenue. Recent commercial development along the corridor has indicated considerable interest from some national chain retailers, such as Home Depot. Since one of this Plan's goals is to link these retail establishments to adjacent neighborhoods and the remainder of the shopping corridor while simultaneously attracting consumers from the surrounding region, it is especially important that compact, walkable centers for shopping and commerce are created and maintained.

An additional strategy of this Plan is the development of smaller neighborhood commercial nodes where small-scale retail and commercial services exist. Careful planning will insure that these hubs arise in strategic locations and provide residents with convenient access to goods and services currently lacking.



Existing neighborhood commercial corridor where small-scale retail should be concentrated.

This Plan also endeavors to provide affordable housing choices for residents of all income levels within the Study Area, including low-income, public housing, workforce and market rate housing. This Plan provides for continued housing construction within the Study Area in designated Residential and Parkfront Residential Districts, which will attract people of all incomes to create a lively, integrated and cohesive community. The design standards set forth in Section 4 of this Plan will ensure that new construction within the Study Area is of high quality and design while helping to create a unique sense of place within the KBS neighborhood which is both attractive and safe.

Numerous public and semi-public spaces exist throughout the Redevelopment Plan Area which can be utilized to a greater degree as community facilities, including schools, places of worship, Woodland Cemetery and other places where people of the community congregate.

West Side Park is currently the primary open space within the Study Area, and is a tremendous asset that has seen an influx of redevelopment activity in recent years. This resource is currently a community meeting place and should continue to be into the future. The possible restoration of Woodland Cemetery, currently in near-abandoned condition, offers another opportunity to create a quality open space within the community.



Finally, the potential exists for undersized City-owned vacant lots throughout the Redevelopment Plan Area to be converted to new green space, community gardens, and playgrounds for all members of the KBS community to enjoy.

In addition to the goals stated above, the unusual intersections created by Springfield Avenue offer opportunities for the creation of unique public open spaces to enhance the built environment in the form of public monuments, unique residential developments patterns or perhaps short pedestrian-only streets that contribute to a vibrant, pedestrian-oriented neighborhood.

The rehabilitation of Woodland Cemetery offers an opportunity for additional open space.

Land Use Categories

To facilitate the creation of the community called for in the Redevelopment Plan Vision, five different zoning Districts have been developed to reflect those uses envisioned and necessary to create a safe, walkable mixed-use neighborhood. Each of the five use Districts contains a predominant use or group of uses (i.e., residential, community, neighborhood or regional commercial, etc.) that will define each District and contribute to and complement the entirety of the neighborhood. Each District is suitable and appropriate to the existing neighborhood fabric and contains its own urban design standards, laid out in Section 4.

Importantly, uses on properties for which the zoning designation is changed are entitled to continue as legal non-conforming uses as a matter of law, regardless of any change in the zoning designation. It is only when these property owners seek to change the existing use to another use that the new zoning standards will apply.

The intent and make-up of each District is as follows:

Residential (R)

The R District occupies the largest portion of the Study Area, as it is currently predominated by residential uses. Its purpose is to ensure sustainable quality infill housing within the Study Area that is complementary and compatible to existing housing, and anticipates a mix of one, two- and three-family homes, based on lot size. Adaptive reuse of existing significant structures is permitted within this District as well. The guidelines provided aim to provide attractive housing choices in detached and semi-detached houses and townhouses with parking provided in the rear, whenever possible.

Parkfront Residential (PR)

The PR District is comprised of lots immediately surrounding West Side Park and provides the same attractive housing choices as the Residential District. This District, however, also permits multifamily mid-rise apartment buildings to front West Side Park, taking advantage of the Park's ability to accommodate increased density with its more than 30 acres of open space. The additional density and activity created by this District assists in addressing public safety issues within the community by putting extra "eyes on the park."

Neighborhood Commercial (NC)

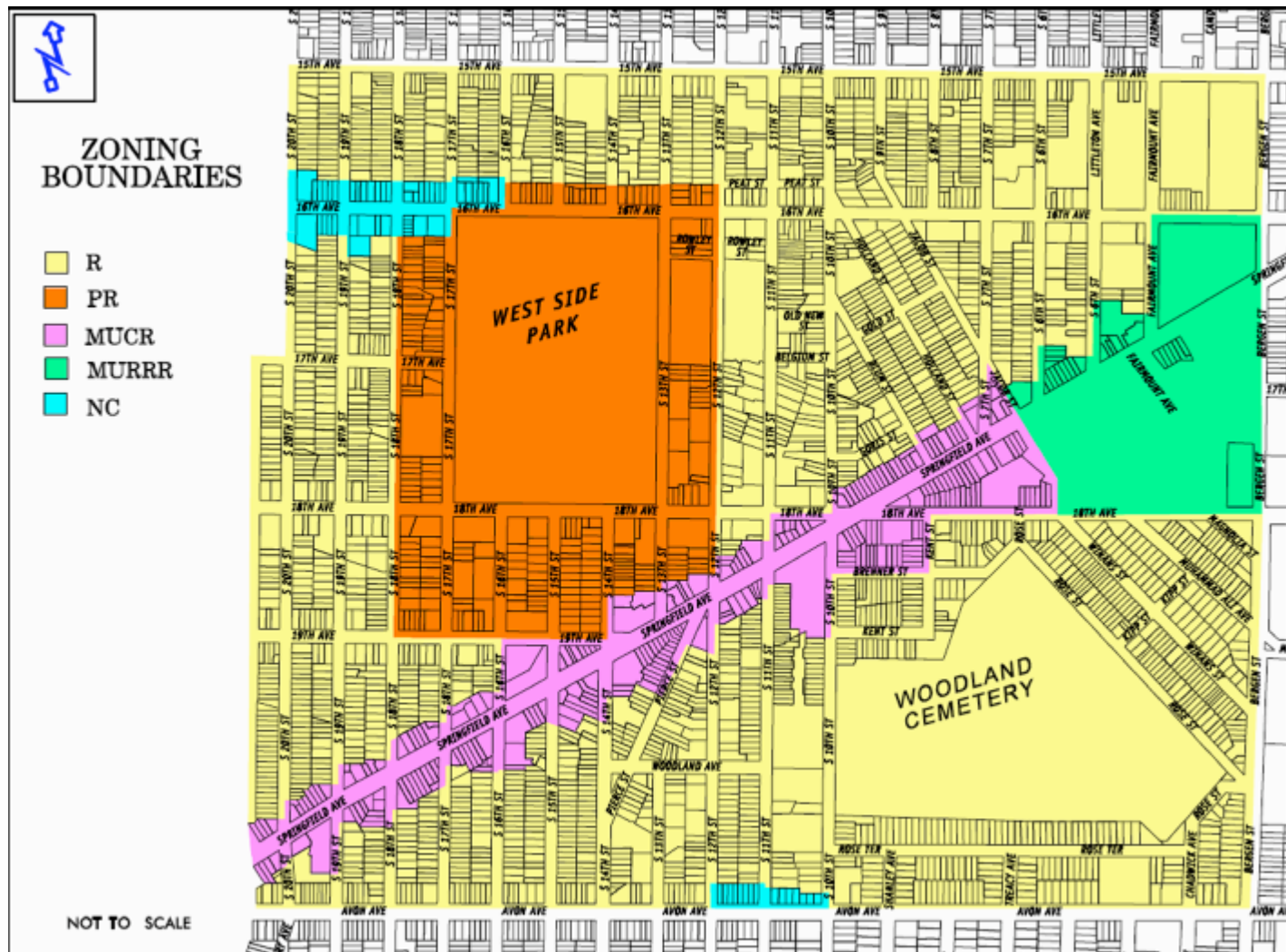
The NC District aims to promote and preserve small-scale convenience shopping and local business services for people who live and work in the neighborhoods in which these businesses are located. Development is allowed up to the street line and attached buildings are permitted. The second and third floors of buildings may accommodate small commercial uses or residential apartments. Parking would be provided on-street; no off-street parking would be required for the retail uses. This category recognizes the existing retail and commercial areas along 16th Avenue, and between 17th and 19th Streets, as well as along Avon Avenue. Existing mixed-use buildings with neighborhood commercial uses on the ground floor are to be maintained under the guidelines of this District.

Mixed-use Commercial Residential (MUCR)

The MUCR District allows for a mix of uses commonly found along community shopping streets in mixed-use developments, including commercial, retail and residential uses. This category is comparable to the existing Community Commercial land use classification in the 2004 Master Plan Land Use Element, as well as the current B2 Zoning District. This District will not, however, permit residential uses that do not have retail or commercial on the ground floor, encouraging a completely mixed-use neighborhood. This District also does not allow for suburban style "big box" development, but rather is oriented more towards neighborhood commerce.

Mixed-use Regional Retail Residential (MURRR)

The MURRR District acknowledges the presence of the regional retail development pattern at the Home Depot and movie theater sites along the eastern end of Springfield Avenue while simultaneously providing for an urban style, mixed-use approach to future development. This District provides for future infill development of the remaining undeveloped pads around the current development to encourage a pedestrian-oriented shopping experience. This category is comparable to the existing Regional Commercial land use classification in the 2004 Master Plan Land Use Element. Mid-rise residential structures will also be permitted on the same lots as retail in the District.



Zoning map representing the five new use Districts within the KBS Study Area



Examples of large format uses interspersed with smaller retail establishments as envisioned within the MURRR District.

Properties zoned for residential use under the new zoning standards may nonetheless be used for commercial uses if the structure ultimately built upon the non-commercial zoned property at least partially fronts Springfield Avenue, and its retail entrance fronts Springfield Avenue. The ultimate decision on whether the proposed zoning change conforms to the conditions detailed above rests with the Central Planning Board.

3.2 Land Use and Development Regulations

3.2.1 Permitted Land Uses and Bulk Requirements

The Land Use and Street Plan assigns land use categories to the various parcels within the Redevelopment Plan Area and provides a general accounting of the permitted uses within each new District, as identified in this Redevelopment Plan Area. All uses are subject to the requirements of the Redevelopment Plan Design Standards in Section 4 of this Plan, the Newark Zoning Ordinance, or as otherwise stated in this Plan. Uses permitted by specific provision of the Municipal Land Use Law are also permitted in the KBS Redevelopment Plan Area.

The uses and design standards presented here are consistent with the intent of the South Ward and Central Ward Plans, the Springfield Avenue Smart Growth Handbook, and the Land Use Element of the Newark Master Plan adopted in 2004. New standards were also created, where required, in order to facilitate a comprehensive approach to planning and development within this Redevelopment Plan Area. All new construction and alterations to existing uses and structures in the Redevelopment Plan Area shall conform to the following requirements, as well as the design requirements in Section 4 of this Plan.

Table 3.1: Permitted Used by District

	Residential (R)	Parkfront Residential (PR)	Mixed-use Commercial Residential (MUCR)	Mixed-use Regional Retail-Residential (MURRR)	Neighborhood Commercial (NC)
Permitted Principal Uses					
Single Family Dwelling	X	X			
Two Family Dwelling	X	X			
Three Family Dwelling (on lots 3500 square feet and greater)	X	X			
Townhouses (on presubdivided lots 7500 square feet or greater)	X	X			
Mid-rise apartment buildings (on lots 5000 square feet or greater)		X			
Mid-rise residential apartment buildings				X	
Dwellings above ground floor retail, office or commercial uses			X	X	X
Child Care Center	X	X	X	X	X
Adult Day Care Center	X				X
School	X	X		X	X
Parks / Playgrounds / Open Space	X	X	X	X	X
Places of Worship	X	X			
Philanthropic and Charitable Uses					X
Places of Worship, Cultural facilities, Philanthropic and Charitable Uses on second floor or above			X	X	X
Art Studios and Galleries			X	X	X
Live/Work Space			X		X
Medical Offices			X		X
Production or assembly use on the second floor and above that makes or assembles products for the retail storefront			X		X

Professional and Government office uses			X	X	X
Retail and Service					X
Restaurants, excluding drive-thrus			X		X
Restaurants, with drive-thrus				X	
Utility, Local/Regional			X		X
Parks / Green Plazas			X		
Financial establishments			X	X	
Schools above the second floor			X		
Movie Theaters				X	
Indoor Amusement Centers				X	
Office			X		X
Office above first floor				X	
Live Entertainment / Dancing				X	
Hotel / Motel				X	
Any combination of uses within one structure in one individual district			X	X	X
Permitted Accessory Uses					
Residential detached parking garages	X	X			
Parking lots and structures				X	
Parking lots for child care centers, schools and places of worship	X	X			
Parking lots and structures, provided no building be set back with its parking lot in front			X	X	

In addition, Tables 3.2, 3.3 and 3.4 detail the bulk standards that apply to development in each of the five individual Districts:

Table 3.2: Schedule of Regulations for New Commercial/Mixed-use Development

Minimum/Maximum	Mixed-use Commercial-Residential (MUCR)	Neighborhood Commercial (NC)	Mixed-use Regional Retail Residential (MURRR)
Min. lot area	1000 square feet (when dwellings are included in the improvements to the zoning lot above the ground floor)	1000 square feet (when dwellings are included in the improvements to the zoning lot above the ground floor)	5,000 square feet (non-residential uses)
Min. lot width	25 feet	25 feet	50 feet (non-residential uses) See Table 3 for Residential Standards
Min. front yard	None	None	None
Maximum front yard	5 feet	5 feet	5 feet
Min. corner side yard	10 feet (when abutting a residential district); None (when abutting a non-residential district)	0 feet minimum/5 feet max	25 feet (abutting a residential district); 5 feet (abutting a non-residential district)
Min. interior side yard	0 feet minimum/5 feet maximum	0 feet minimum/5 feet maximum	0 feet minimum/5 feet maximum
Min. rear yard	25 feet (when abutting a residential district); 20 feet (when abutting a non-residential district)	25 feet (when abutting a residential district); 20 feet (when abutting a non-residential district)	20 feet
Max. building height	5 stories at front façade, up to 7 stories with 15 feet minimum step back per additional story	4 stories or 49 feet (excluding basements and cellars)	5 stories at front façade, up to 7 stories with 15 feet minimum step back per additional story
Minimum building height (ask about measuring height)	3 stories or 35 feet	3 stories or 35 feet	1 story or 20 feet
Minimum required parking – Retail	One space per 1,000 square feet of gross floor space, excluding the first 5,000 square feet	One space per 1,000 square feet of gross floor space, excluding the first 5,000 square feet	One space per 1,000 square feet of gross floor space, excluding the first 5,000 square feet
Minimum required parking - Office	1,500 square feet of gross floor space, excluding the first 5,000 square feet	1,500 square feet of gross floor space, excluding the first 5,000 square feet	1,500 square feet of gross floor space, excluding the first 5,000 square feet
Minimum required parking - Commercial	2,500 square feet of gross floor space, excluding the first 5,000 square feet	2,500 square feet of gross floor space, excluding the first 5,000 square feet	2,500 square feet of gross floor space, excluding the first 5,000 square feet
Minimum required parking – Residential	0	N/A	0

Table 3.4: Schedule of Regulations for New Residential District Development

	One and two-family dwellings	Three-family dwellings	Townhouses	Child Care Center (New)	Adult Day Care	House of Worship	Schools
Minimum lot area	2500 sq feet*	3500 sq feet	7500 sq feet (pre-subdivision)	3500 sq feet	5000 sq feet	5000 sq feet	See Newark Zoning Ordinance, B-2 regulations
Minimum lot width	25 feet	35 feet	75 feet	35 feet	50 feet	50 feet	
Minimum front yard	4 feet	4 feet	4 feet	6 feet	6 feet	10 feet	
Maximum front yard	6 feet	6 feet	6 feet	6 feet	6 feet	10 feet	
Minimum side yard	0 minimum / 6 feet combined. For a duplex attached on one side for two lots with a minimum of 5,000 combined: 0 minimum / 3 feet combined per lot	0 minimum / 6 feet combined. For a duplex attached on one side for two lots with a minimum of 5,000 combined: 0 minimum / 3 feet combined per lot	0 minimum / 6 feet combined at ends of townhouse row	6 feet combined	10 feet combined	6 feet combined	
Minimum rear yard	30 feet	30 feet	30 feet	30 feet	20 feet	30 feet	
Maximum building height	3 stories, 35 feet	3 stories, 35 feet	4 stories, 40 feet	3 stories, 35 feet	3 stories, 35 feet	3 stories, 35 feet	
Parking minimum	1 space per unit	1 space per unit	1 space per unit	1 space per caregiver	1 space per caregiver	1 space per 10 seats, with the first 50 seats exempt	
* Lots of record that are below 2500 square feet may be developed with a one family dwelling unit only ** See Section 4 for additional requirements							

Table 3.5: Schedule of Regulations for New Parkfront Residential Development

	One and two-family dwellings	Three-family dwellings	Mid-rise apartments	Townhouses	Child Care Center (New)	House of Worship	Schools
Minimum lot area	2500 sq feet*	3500 sq feet*****	5000 sq feet	7500 sq feet (pre-subdivision)	3500 sq feet	5000 sq feet	See Newark Zoning Ordinance, B-2 regulations
Minimum lot width	25 feet	35 feet	50 feet	75 feet	35 feet	50 feet	
Minimum front yard	4 feet	4 feet	5 feet	4 feet	6 feet	10 feet	
Maximum front yard	6 feet	6 feet	10 feet	6 feet	6 feet	10 feet	
Minimum side yard	0 minimum / 6 feet combined. For a duplex attached on one side for two lots with a minimum of 5,000 combined: 0 minimum / 3 feet combined per lot	0 minimum / 6 feet combined. For a duplex attached on one side for two lots with a minimum of 5,000 combined: 0 minimum / 3 feet combined per lot	10 feet combined	0 minimum / 6 feet combined at ends of townhouse row	6 feet combined	6 feet combined	
Minimum rear yard	30 feet	30 feet	20 feet	30 feet	30 feet	30 feet	
Maximum building height	3 stories, 35 feet	3 stories, 35 feet	5 stories, 60 feet**	4 stories, 40 feet	3 stories, 35 feet	3 stories, 35 feet	
Parking minimum	1 space per unit	1 space per unit	1 space per unit***	1 space per unit	1 space per caregiver	1 space per 10 seats, with first 50 seats exempt	
<p>* Lots of record that are below 2500 square feet may be developed with a one family dwelling unit only</p> <p>** Fifth floor must be stepped back 10 feet from the front of the building</p> <p>*** Parking must not be visible from street</p> <p>**** See Section 4 for additional requirements</p> <p>***** Three-unit dwellings are permitted on lots of a least 35 feet in width and 3500 square feet in area, or as a duplex, attached on one side, on two lots totaling 5000 square feet</p>							

Additional Regulations and Requirements

In addition to the preceding standards and guidelines directing development above, the following regulations and requirements shall also apply to new development within the Study Area.

(1) Rehabilitation and adaptive reuse

The Redevelopment Plan further allows for the rehabilitation of existing buildings for residential, commercial or mixed residential/commercial purposes. Adaptive reuse is encouraged where appropriate, particularly for buildings with historic character.

(2) Minimum Dwelling Unit Size for New Construction and Rehabilitation

New residential units created from rehabilitation of an existing non-residential structure under this Plan shall provide for a minimum area per unit of:

1 Bedroom	700 sq. feet
2 Bedrooms	900 sq. feet
3 Bedrooms	1,200 sq. feet
4 Bedrooms	1,500 sq. feet
5 Bedrooms	1,700 sq. feet

The minimum area requirements above are equal to, and in most cases greater than, those required by the State of New Jersey Housing & Mortgage Finance Agency standards.

2. At a minimum, one bedroom in each unit must be at least 150 sq. feet, not including closets. Remaining bedrooms must be at least 100 sq. feet, not including closets.

(3) Density

Existing in-fill lots under 2,500 square feet will be allowed a maximum of one unit. For existing buildings being rehabilitated with the same use, the maximum number of units shall be the same as previously recorded at the tax assessor's office, or as called for in the minimum dwelling unit sizes listed in (6) below. For buildings being adaptively reused from a use other than residential, the maximum density shall be governed by the minimum dwelling unit sizes specified in this Plan.

(4) Setback

In addition to the setback requirements of this Plan, no building may be constructed such that it would encroach within 3.2 feet of, or block access to, the windows of any neighboring building.

(5) Fences

All fences located at the public right-of-way shall be transparent, shall be constructed of decorative metal or wood, and shall not exceed 4 feet in height. Fencing in the rear yard of a residential lot shall not exceed 6 feet in height, can be opaque or transparent, and can be made of metal, wood or vinyl. Barbed, razor, chain link, or equivalent fencing is not permitted under this Plan.

(6) Parking and Loading

Parking for residential uses shall be provided at a ratio of not less than 1 parking space per dwelling unit. Parking in the MUCR district shall be located in the rear, or if necessary, the side of the lot. Parking is not required for the rehabilitation of existing buildings of record for the same use. Parking requirements can be satisfied through the use of other properties located within 500 feet of the development site, provided that the parking site is under the same ownership as the development site. Where feasible, all loading shall occur at the rear of buildings.

3.2.2 Industrial Uses

There are several pre-existing industrial facilities located within the Study Area, primarily located in residential districts. Like all pre-existing uses within the Study Area, pre-existing industrial operations are allowed to continue as legal, non-conforming uses in all districts. Should the use of these properties intensify, however, additional parking requirements generated from that use change will need to be provided in off-street parking areas either on-site or off-site, within 500 feet of the site and at a rate of one space per four employees. Adaptive reuse of industrial properties into residential or mixed-use properties is allowed and encouraged under this plan, under guidelines established in section 3.2.1

3.3 Circulation and Transportation Overview

The street pattern within the Study Area consists of a grid of numbered avenues running east and west and numbered streets run north and south. Generally, the even numbered streets are one-way southbound with the odd numbered streets running one-way northbound. The key objectives for improving traffic circulation in the study area are to:

- Include bus-lane or dedicated bus stops within the design of new development to accommodate BRT and/or conventional bus transit so that buses can pull out of the traffic stream on Springfield Avenue
- Provide on-street parking on Springfield Avenue for shoppers whenever possible
- Prohibit curb cuts along Springfield Avenue for better pedestrian safety and circulation
- Provide for traffic calming at intersections by combining devices such as speed tables with pedestrian crossings to increase pedestrian comfort and reduce traffic speeds
- Provide for traffic calming devices and alternate traffic control signage at intersections, as appropriate, along one-way streets that have an uninterrupted run where traffic speeds on residential streets can accelerate (10th Street, 11th Street, 12th Street, 18th Street, 19th Street)

The essential strategy for separating regional traffic traveling to and from Newark employment centers from locally destined traffic is to use the numbered avenues (15th, 16th and 18th Avenues) to access parking areas for anchor stores. Access to the retail centers from Springfield Avenue would be focused on the pedestrian, with prominent pedestrian crossings at the signalized intersections. While the red light cycle on Springfield Avenue may require a longer period of time to facilitate safe and comfortable pedestrian movement, the improved street grid movement from diverting local and destination traffic to the numbered avenues, eliminating double parking, reducing left turn movements and minimizing curb cuts should shorten travel times significantly.

New Jersey Transit launched its pilot Bus Rapid Transit (BRT) system along Springfield Avenue in April of 2008 in response to high ridership on its No. 25 bus route. The pilot program is an express bus service from Irvington Terminal to Newark Penn Station, with special branding colors and bus shelters. There are two stops in the redevelopment area on Springfield Avenue: South 18th Street and South 10th Street which should help support the new development called for in this Plan.

The Springfield-Bergen Redevelopment Area is located in the eastern portion of the Redevelopment Plan Area at the convergence of Springfield and 18th Avenues with Bergen Street and is the site of the Home Depot redevelopment project. This redevelopment plan and acquisition map will supersede the Springfield Bergen Redevelopment Plan and regulate the area previously governed by that Plan.

Section 4. Design Standards

The purpose of this Plan is to create a vibrant, productive, pedestrian-oriented development pattern that enhances public safety, expands housing opportunities, community resources and services, and creates local and regional economic opportunities. The following design standards are intended to help accomplish this goal by maximizing density within the Study Area while creating public and private spaces that define and maintain the neighborhood character and create a distinctive sense of place.

These design standards have also been developed to explicitly address the public safety concerns which arose during multiple community outreach sessions. Specific design improvements and standards have been included to create a safe community for local residents. For example, new retail developments have been required to provide minimum coverage of windows to insure that neighborhood streets remain visible to residents, customers and shopkeepers. Front porches have been encouraged not only to provide semi-public spaces for residents, but to insure that more eyes remain on the street to observe activity taking place within the neighborhood. Further, parking has been moved from the front to the back of structures when feasible to place automobiles in rear yard areas which are less accessible to the public, more private in nature and more likely to single out trespassers. Finally, fencing has been adjusted to the pedestrian scale to insure that it maintains an important barrier between the public and private spheres while not interfering with neighborhood aesthetics.

Due to the relatively large size of the Study Area, it has been divided into five new Districts in order to provide individual design standards that are appropriate for the character and nature of the uses prescribed for each. Each District details the specific lot, construction, landscaping and improvement standards that must accompany development in that zoning area. This section also addresses the streetscape standards that must accompany all new development to create a safe and sustainable pedestrian environment, as well as vehicular and non-vehicular access issues. The section concludes by specifying the standards that must accompany improvements to open spaces and off-site improvements. The following standards contained in the section shall be applied to all development in the KBS Redevelopment Plan Area and shall be considered zoning requirements.

General Requirements

- New buildings shall attempt to complement and enhance the neighborhood architectural style to the greatest degree possible by using features common to the neighborhood such as vernacular architectural trim, front porches, bay windows, and/or similar rooflines in accordance with the remaining architectural style of the neighborhood. Any portion of a site that is not necessary for building or parking purposes shall be appropriately landscaped in a manner consistent with the surrounding neighborhood.
- First floor facades intended for retail use must have large, clear storefront glass areas (70% of the area) to display the nature of the business and produce an interesting streetscape. Large blank walls are prohibited along any street.
- Each street-level shop shall have its own shop front. The shop fronts may either have identical designs to reinforce the building design or varied designs to express the different businesses. A shop front should be separated from the roofline or a second floor by a horizontal architectural element such as a sash, cornice, frieze or molding. Upper floor uses are to be provided with separate exterior entrances unless a large common lobby or atrium is provided.

- For all districts except the MURRR, all buildings are required to have entrances accessed directly from the sidewalk. Sidewalks should extend from the building façade to the curb, for the purpose of facilitating pedestrian movement and creating opportunities for the placement of street furniture for outdoor dining and shopping, transit access, and socializing.
- Fence height should not exceed six feet for rear and side yards. Barbed, razor, or equivalent fencing will not be permitted. All fences at the streetline shall be transparent and shall be constructed of decorative metal or wood, not to exceed 4 feet at the street. If a fence is installed on top of a decorative or retaining wall, the combined height of both the wall and fence may not exceed the permitted fence height for the area.
- Roofs may be flat, but they must include appropriate ornamentation such as cornices or parapets.

4.1 Residential Design Standards

4.1.1 Building Location and Organization

Type 1 Residential - Infill Development



Plan



Summary

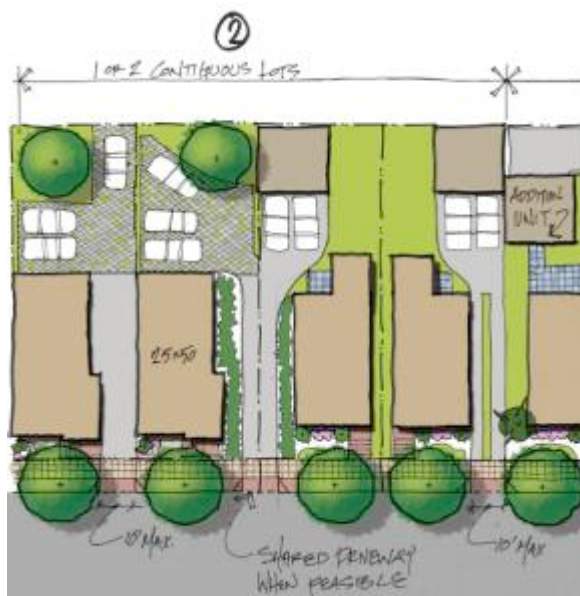
This section addresses infill residential lots that are greater than 18' and less than 35' in width. Since narrow lot widths restrict the ability to provide parking behind new units, lots less than 35' in width are permitted to have a front accessed garage and a single curb cut, provided that they adhere to the guidelines set forth in this Section.

Standards and Guidelines

- Front yards shall be setback a minimum of 4' and a maximum of 6' to provide for a stoop and planting zone.
- Front Yards shall provide a minimum of 55% pervious area, consisting of planting strips and beds. Pervious pavers may be utilized in the pervious area calculation, depending on paver type, at the discretion of the Planning Board.
- Front accessed, attached garage entrances must be setback a minimum of 8' and a maximum of 10' from the property line.
- So as not to be the most prominent feature of dwelling units, front accessed garages must:
 - 1) Be recessed two feet into the front façade wall; or
 - 2) Be located below grade (1/2 story min.)
- Side yards should provide a combined side yard setback of 6'. Zero setback is permitted on one side of the lot provided that any adjacent buildings are greater than 3' from the neighboring principal structure.
- All houses should be setback a minimum of 30' from the rear property line.
- All houses shall have a front door that faces the street.
- Curb cuts and driveways shall be a maximum of 10' wide and are encouraged to incorporate pervious pavers or a center planting strip whenever feasible.
- The front stoop shall not be counted in the front yard setback requirement.
- Residential buildings should not exceed 3 stories or 35', measured from the centerline of the roadway that the building fronts onto the highest point of the roof.
- Architectural elements such as towers and cupolas may break this limit by not greater than 6'.
- Buildings should be designed so that the front door, and not the garage door, is the most prominent element on the street.

Driveway with center planting strips

Type 2 Residential – Detached Housing



Plan



Detached garage with unit above

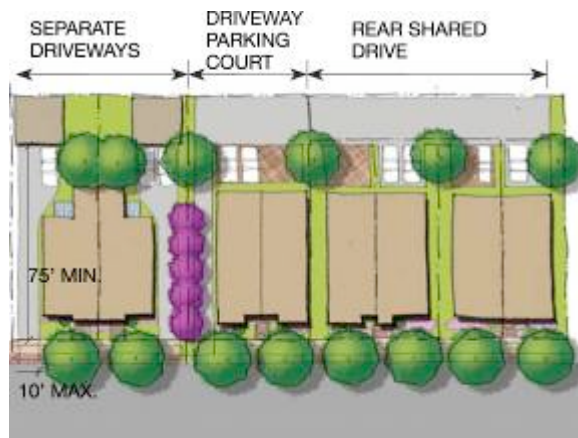
Summary

This section addresses infill residential lots that are greater than 35' and less than 50' in width. This lot width is envisioned to accommodate multi-family housing containing 2-3 units, with parking in the rear.

Standards and Guidelines

- Front yards shall be setback a minimum of 4' and a maximum of 6' to provide for a stoop and planting zone.
- Front-accessed attached garages are not permitted.
- Front Yards shall provide a minimum of 60% pervious area, consisting of planting strips and beds.
- A variety of parking options are acceptable, including rear parking court, shared alley, and attached and detached rear-access garages.
- Detached garages should be set back a minimum of 75' from the front property line.
- An additional housing unit may be provided above the detached garage provided that this unit meets all applicable code requirements and does not exceed 2 stories / 25'.
- Side yards should provide a combined side yard setback of 6'. Zero setback is permitted on one side of the lot provided that any adjacent buildings are greater than 3' from the neighboring principal structure.
- All houses should be setback a minimum of 30' from the rear property line.
- All houses shall have a front door that faces the street.
- Driveways should be a maximum of 10' wide and are encouraged to incorporate pervious pavers or a center planting strip.
- Shared driveways are encouraged to maximize the efficiency of narrow lots.
- Whenever possible, a minimum of 18' shall be provided between driveways and curb cuts in order to allow for on-street parking. If 18' is not available, a minimum of 8' shall be provided and this space should be landscaped with low shrubs and trees in order to properly screen the driveways.
- One of the permitted units on the lot may be located above the detached garage. The garage may not exceed 25 feet in height and there shall be no windows on the rear façade, and less than 3 feet from the side property line.
- Residential buildings should not exceed 3 stories or 35', measured from the centerline of the roadway that the building fronts on to the highest point of the roof.
- Elements such as towers and cupolas may break this 45' limit by not greater than 6'.

Type 3 Residential - Semi-Detached Housing



Plan



Multi-family attached homes



Summary

This section addresses infill residential lots that are greater than 50' and less than 70' in width. This lot width is envisioned to accommodate a semi-attached multi-family home containing up to six units per attached structure. These homes should maximize density while maintaining the scale and appearance of other homes in the neighborhood. Units may be combined to read as one "larger" home or to read as 2 separate homes depending on the size and existing neighborhood fabric.

Standards and Guidelines

- Front yards shall be setback a minimum of 4' and a maximum of 6' to provide for a stoop and planting zone.
- Front-accessed attached garages are not permitted
- All on-site parking must be rear accessed and shared driveways are encouraged.
- Rear shared "parking drives" are required for this lot size in order to maximize efficiency of narrow lots
- A variety of parking options are acceptable, including rear parking court, shared alley, and attached and detached rear-access garages.
- Detached garages should be set back a minimum of 75' from the front yard. If lot depth does not allow for this setback, the garages should be located so that the rear of the garage is not greater than 5' from the rear property line.
- Whenever possible, a minimum of 18' shall be provided between driveways and curb cuts in order to allow for on-street parking. If 18' is not available, a minimum of 8' shall be provided and this space should be landscaped with low shrubs and trees in order to properly screen the driveways. Care should be taken regarding the height of this plant material so that it does not obscure sight lines for cars entering and exiting the driveway.
- An additional housing unit may be provided above the detached garage, provided that this unit meets all applicable code requirements and that this structure does not exceed 2 stories or 25' in height.
- Side yards should provide a combined side yard setback of 6'. Zero setback is permitted on one side of the lot provided that any adjacent buildings are greater than 3' from the neighboring principal structure.
- All houses should be setback a minimum of 30' from the rear property line.
- All houses shall have a front door that faces the street.
- Driveways should be a maximum of 10' width and are encouraged to incorporate pervious pavers or a center planting strip whenever feasible.
- Shared driveways are encouraged to help maximize efficiency of two adjacent narrow lots.

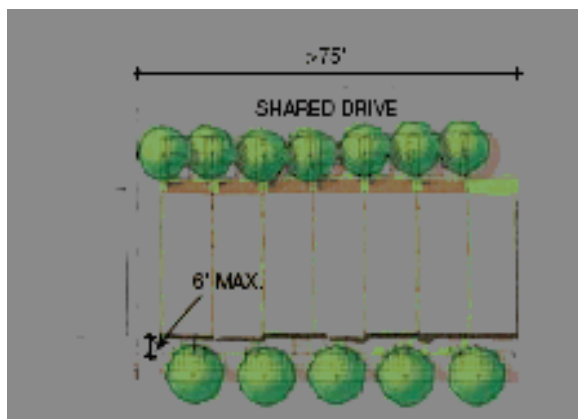
Typical attached multi-family homes



Typical rear accessed parking from shared drive

- Residential buildings should not exceed 3 stories or 35', measured from the centerline of the roadway that the building fronts on to the highest point of the roof.
- Architectural elements such as towers or cupolas may break this 45' limit by not greater than 6'.

Type 4 Residential - Townhouses



Plan



Contemporary row houses

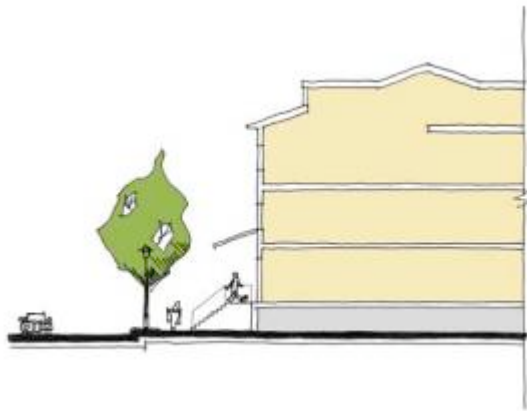
Summary

This section addresses infill residential lots that are greater than 70' in width. This lot width is envisioned as accommodating attached single, two- or three-family townhouses. These homes should maximize density while maintaining the scale and appearance of other homes in the neighborhood.

Standards and Guidelines

- Front yards shall be setback a minimum of 4' and a maximum of 6' to provide for a stoop and planting zone.
- Front accessed attached garages are not permitted.
- A shared "parking drive" should be provided to access rear garages, parking courts or other rear parking solutions.
- Whenever possible, a minimum of 18' shall be provided between driveways and curb cuts in order to allow for on-street parking.
- Side yards should provide a combined side yard setback of 6' at the end of the row. Zero setback is permitted on one side of the lot provided that any adjacent buildings are greater than 6' away from the neighboring principal structure.
- All houses should be setback a minimum of 30' from the rear property line.
- All houses shall have a front door that faces the street.
- Shared drives should be a maximum of 10' width for one-way traffic and 18' width for two-way traffic. Pervious pavers or a center planting strips should be provided whenever feasible.
- Front yards shall provide a minimum of 60% pervious area, consisting of planting strips and beds
- Residential buildings should not exceed 4 stories or 40', measured from the centerline of the roadway that the building fronts on to the highest point of the roof.
- Architectural elements such as towers or cupolas may break this 45' limit by not greater than 6'.

4.1.2 Building Form



Front door as most prominent element

Summary

The section ensures that the scale, setbacks and height of new residential infill developments maximize density while allowing for proper air and light. It is the intent of this plan to maintain the existing “low-scale” residential character of these existing neighborhoods.

Standards and Guidelines

- Residential buildings should not exceed 3 stories or 35', measured from the centerline of the roadway that the building fronts on to the highest point of the roof.
- Architectural elements such as towers and cupolas may break this 45' limit by not greater than 6'.
- Buildings should be designed so that the front door, and not the garage door, is the most prominent element on the street. When front accessed garages are provided, the garage should be recessed a minimum of 3' from the front door.

4.1.3 Building Face



Appropriate use of window openings



Bays and tower elements articulate façade

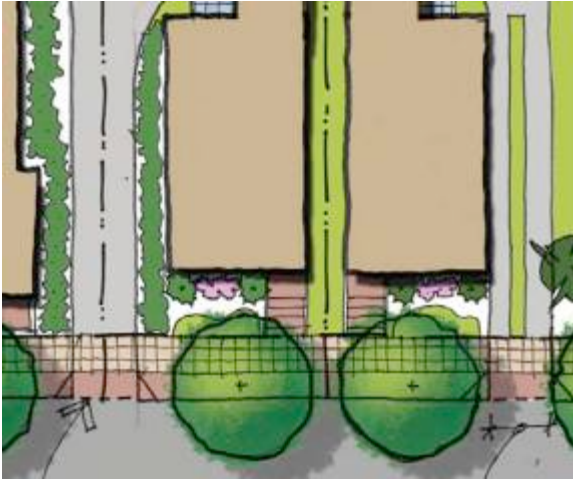
Summary

This section is intended to challenge and guide architects and developers to create high quality design and distinctive buildings that integrate well into the fabric of the neighborhood. It is important that the design of individual facades is not repetitive, but rather compliment one other to create interesting and aesthetically pleasing street edges.

Standards and Guidelines

- A minimum of 30% of the front elevation of all residential buildings shall be windows and openings.
- Elevations should have distinct and appropriate architectural character.
- Non-textured vinyl siding should be limited to 1/3 of the total façade area, excluding area for windows and openings.
- Synthetic stucco (EIFS) shall not be permitted on any portion of the front facade.
- Corner buildings should appropriately treat both street fronting facades per these guidelines.
- Corner buildings should be architecturally memorable and distinctive and are encouraged to incorporate towers, corner bays and/or gables.
- Front doors should always face the street and should be a design element of the facade.
- Buildings that “terminate views” should be designed to be memorable and distinctive are encouraged to incorporate towers, bays and other distinctive features.
- Facade should be properly articulated and should not be “flat”, single plane elevations. The proper use of bays, porches and balconies is encouraged to help articulate the facade.

4.1.4 Site Landscaping



Plan



Decorative "green" driveway treatment

Summary

The front yard and streetscape in front of each home is very important in creating a safe, aesthetically pleasing neighborhood environment.

Standards and Guidelines

- Decorative "green" driveway treatments are encouraged.
- Driveways, excluding driveway apron and sidewalk areas, shall be constructed using decorative pavement materials from, at a minimum, the front property line to the front façade of the building. Materials including decorative concrete, concrete pavers, brick, permeable pavers and other permeable surfaces (planting strips) are encouraged.
- Driveways shall not be wider than 10' except for shared drives used to access rear parking.
- Each new house should have a planting zone located between the curb and sidewalk containing hardy ground covers, a street tree and low shrubs or perennials. Planting zones shall be a minimum of 4' in width and 6' in length. Refer to Section 4.6 for additional detail.
- Whenever possible, this planting zone should be a continuous planting strip containing street trees spaced at a maximum of 30' on center.
- Front yards of all residential properties should be green "landscaped areas" containing ground covers, low shrubs and trees.
- Rear yards of all residential properties should not be 100% impervious surface; areas not utilized for parking should be covered with groundcover, grass, pervious pavers or other pervious material.

4.2 Parkfront Residential (PR) Design Standards

4.2.1 Building Location and Organization



Summary

The building location and organization, building form, building face, and landscaping standards for the Parkfront Residential District shall be identical to those detailed for the Residential District for the housing types outlined in section 4.1. In addition, mid-rise residential buildings are permitted and must adhere to the following standards:

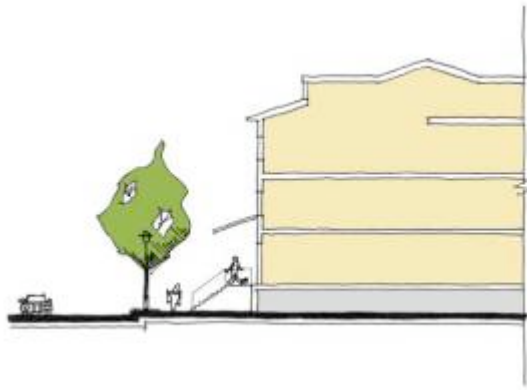
Standards and Guidelines

- Multi-family buildings are permitted on lots 50' or wider.
- Building height shall be four stories in height or 49', or five stories with a setback of 15 feet from the front streetline.
- Front yards shall be setback a minimum of 5' and a maximum of 10' to provide for a stoop and planting zone.
- Front-accessed attached garages are not permitted.
- A "parking drive" should be provided to access rear garages, parking courts or other rear parking solutions.
- Whenever possible, a minimum of 18' shall be provided between driveways and curb cuts in order to allow for on-street parking.
- Side yards should provide a combined side yard setback of 10 feet combined.
- All houses should be setback a minimum of 20' from the rear property line.
- All houses shall have a front door that faces the street.
- Shared drives should be a maximum of 10' wide for one-way traffic and 18' wide for two-way traffic. Pervious pavers or a center planting strip should be provided whenever feasible.
- Front yard shall provide a minimum of 60% pervious area consisting of planting strips and beds



Modern 4-story multi-family buildings

4.2.2 Building Form



Front door as most prominent element

Summary

The section ensures that the scale, setbacks and height of new residential infill developments maximize density while allowing for proper air and light. It is the intent of this plan to maintain the existing “low-scale” residential character of these existing neighborhoods.

Standards and Guidelines

- Residential buildings should not exceed 5 stories or 60 feet measured from the centerline of the roadway that the building fronts on to the highest point of the roof.
- Architectural elements such as towers and cupolas may break this 60’ limit by not greater than 6’.
- Buildings should be designed so that the front door, and not the garage door, is the most prominent element facing the street.

4.2.3 Building Face



Appropriate percentage of window openings

Summary

This section is intended to challenge and guide architects and developers to create high quality design and distinctive buildings that integrate well into the fabric of the neighborhood. It is important that the design of individual facades is not repetitive, but rather compliment one other to create interesting and aesthetically pleasing street edges.

Standards and Guidelines

- A minimum of 30% of the front elevation of all residential buildings shall be windows and openings.
- Elevations should have distinct and appropriate architectural character.
- Non-textured vinyl siding should be limited to 1/3 of the total façade area, excluding area for windows and openings.
- Synthetic stucco (EIFS) shall not be permitted on any portion of the front façade or on the first story level.
- Corner buildings should appropriately treat both street fronting facades per these guidelines.
- Corner buildings should be architecturally memorable and distinctive and are encouraged to incorporate towers, corner bays and/or gables.
- Front doors should always face the street and should be a design element of the facade.
- Buildings that “terminate views” should be designed to be memorable and distinctive and are encouraged to incorporate towers, bays, and other distinctive features.
- Façade should be properly articulated and should not be “flat”, single plane elevations. The proper use of bays, porches, and balconies is encouraged to help articulate the facade.

4.2.4 Site Landscaping



Decorative “green” driveway treatment.

Summary

The front yard and streetscape in front of each building is very important in creating a safe, aesthetically pleasing neighborhood environment.

Standards and Guidelines

- Decorative “green” driveway treatments are encouraged.
- Driveways, excluding driveway apron and sidewalk areas, shall be constructed using decorative pavement materials from, at a minimum, the front property line to the front façade of the building. Materials including decorative concrete, concrete pavers, brick, permeable pavers and other permeable surfaces (planting strips) are

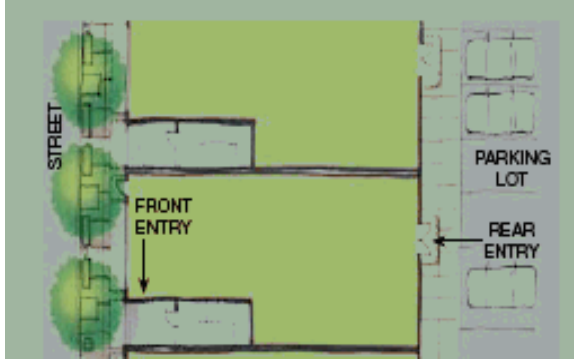


Planting strip with a variety of vegetation

- encouraged.
- Driveways shall not be wider than 10' except for shared drives used to access rear parking.
- Each new building should have a planting zone located between the curb and sidewalk containing hardy ground covers, a street tree and low shrubs or perennials.
- Planting zones shall be a minimum of 4' in width and 6' in length. Refer to Section 4.7 for additional detail.
- Whenever possible, this planting zone should be a continuous planting strip containing street trees spaced at a maximum of 30' on center.
- Front yards of all residential properties should be green "landscaped areas" containing ground covers, low shrubs and trees.

4.3 Neighborhood Commercial (NC) Design Standards

4.3.1 Building Location and Organization



Typical plan

Summary

This section seeks to maintain the existing historic character of the corridor while allowing for suitable “convenience retail” to become established as the neighborhood evolves.

Standards and Guidelines

- Front façade of buildings should be located at the property line with a zero setback.
- Off-street parking is not required.
- There should be no side yards to ensure a continuous retail “street wall”.
- All retail and lobby spaces shall be accessed directly from the sidewalk in front of the building and shall be clearly visible from the street.



Typical 4-story neighborhood commercial

4.3.2 Building Form



Typical section



Stepping back upper floors to allow for proper street scale

Summary

This section ensures that the scale, setbacks and height of new commercial mixed-use developments maximize density while allowing for proper air and light. It is the intent of this plan to maintain the existing “low-scale” neighborhood character of commercial space that is not located along the Springfield Avenue corridor.

Standards and Guidelines

- Neighborhood commercial buildings should not exceed 4 stories or 49’ measured from the centerline of the roadway that the building fronts onto the highest point of the roof.
- Architectural elements such as parapet walls, towers and cupolas may break this 49’ limit by not greater than 6’.
- Buildings that “step back” from the street at the upper levels are encouraged in order to provide greater light and air. Step backs are not, however, required.

4.3.3 Building Face



Primary storefront with glass façade

Summary

This section is intended to challenge and guide architects and developers to create high quality design and distinctive buildings that integrate well into the fabric of the neighborhood. It is important that the design of individual facades is not repetitive, but rather compliment one other to create interesting and aesthetically pleasing street edges.

Standards and Guidelines

- A minimum of 70% of primary storefront facades shall be glass.
- Upper floors should provide an appropriate balance between openings and solid surfaces so as to create an architecturally pleasing building elevation.
- Elevations should be designed to be distinctive and interesting while complimenting existing adjacent architecture.
- Signs and awnings shall adhere to the requirements set forth in Section 4.8

4.3.4 Site Landscaping



Parking lot containing impervious planting zone

Summary

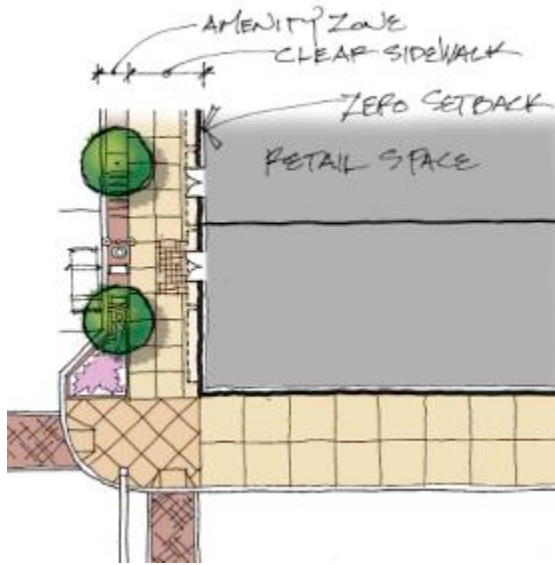
The streets fronting Neighborhood Commercial Districts provide access and circulate traffic, but also act as the social hubs of the neighborhood. It is therefore important that the landscaping in these sites accommodate this variety of uses. Public safety and welfare, retail visibility and aesthetics must be integrated to create a well-balanced street.

Standards and Guidelines

- If the rear yard is not utilized for parking, then it should not be 100% impervious surface; it should be covered with groundcover, grass, pervious pavers or other pervious material.

4.4 Mixed-use Commercial-Residential (MUCR) Design Standards

4.4.1 Building Location and Organization



Plan

Summary

This section seeks to maintain the existing, historic character of the corridor and is intended to assist developers in creating mixed-use buildings that maintain the traditional “zero lot line” urban street wall while addressing current and future needs of regional retailers, including increased ceiling heights for interior display and parking.

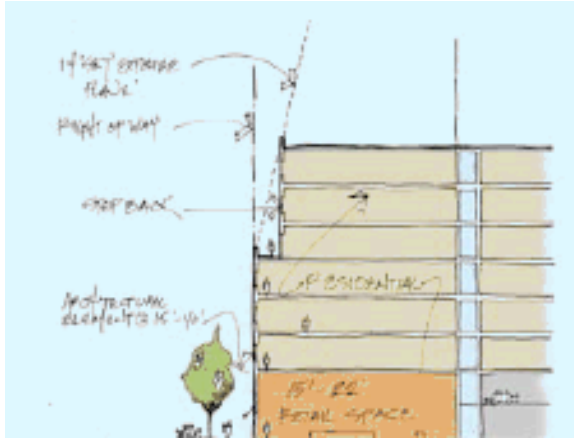
Standards and Guidelines

- Front façade of buildings should be located at the property line with “zero setback” condition.
- All off-street parking shall be accommodated in shared parking facilities located behind the building.
- Parking structures shall be constructed so as to minimize or completely eliminate visibility from Springfield Avenue and adjacent residential neighborhoods.
- There should be no side yards so as to ensure a continuous retail “street wall”.
- All retail and lobby spaces shall be accessed directly from the sidewalk in front of the building and shall be clearly visible from the street.



Mixed-use building with retail fronting sidewalk

4.4.2 Building Form



Typical section



Stepback & corner element

Summary

This section is intended to guide developers in constructing buildings that achieve high density while respecting the traditional 5-story scale of Springfield Avenue. With the use of a building envelope established through setbacks and building height stepbacks, these guidelines allow buildings to achieve higher densities, provide flexibility as the real estate market evolves and ensures ample air and light for all residents.

Standards and Guidelines

- Buildings can be up to 5 stories or 60' at the property line, and up to 7-stories with a 15 foot step stepback above the fifth story.
- Balconies, parapet walls and other architectural elements are permitted to project above the fifth floor.
- Buildings that “step back” from the street at the upper levels are encouraged in order to provide greater light and air. Stepbacks are not, however, required below 60'.
- Architectural elements such as cornices and balconies shall be located at varying heights between 15' and 40' in order to reduce the perceived scale of the building and mimic the historic varied building heights along Springfield Avenue.

4.4.3 Building Face



Continuous glass retail storefront



Glass storefront

Summary

This section is intended to challenge and guide architects and developers to create high quality design and distinctive buildings that integrate well into the fabric of the neighborhood. It is important that the design of individual facades is not repetitive, but rather compliment one other to create interesting and aesthetically pleasing street edges.

Standards and Guidelines

- New commercial buildings must have large continuous glass storefronts facing the sidewalk and, where appropriate, facing their adjoining parking lots. Large blank walls are not acceptable. A minimum of 70% of primary storefront facades shall be glass.
- Upper floors should provide an appropriate balance between openings and solid surfaces as to create an architecturally pleasing building elevation.
- Elevations should be designed to be distinctive and interesting while complimenting existing adjacent architecture.

4.4.4 Site Landscaping



Parking lot containing impervious planting zone

Summary

The streets fronting Mixed-use Commercial Residential Districts provide access and circulate traffic, but also act as the social hubs of the neighborhood. It is therefore important that the landscaping in these sites accommodate this variety of uses. Public safety and welfare, retail visibility and aesthetics must be integrated to create a well-balanced street.

Standards and Guidelines

- If the rear yard is not utilized for parking, then it should not be 100% impervious surface; it should be covered with groundcover, grass, pervious pavers or other pervious material.

4.5 Mixed-use Regional Retail-Residential (MURRR) Design Standards

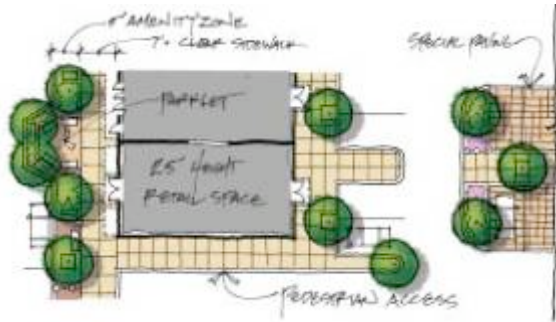
4.5.1 Building Location & Orientation

Summary

This section is intended to help assist developers in creating a shopping center environment that is desirable to regional retail tenants while also creating a pedestrian-friendly urban environment. Many of the guidelines here address infill options that are intended to assist in “re-pedestrianizing” the current shopping center and new potential development sites that exist or may be assembled in the future.

Standards and Guidelines

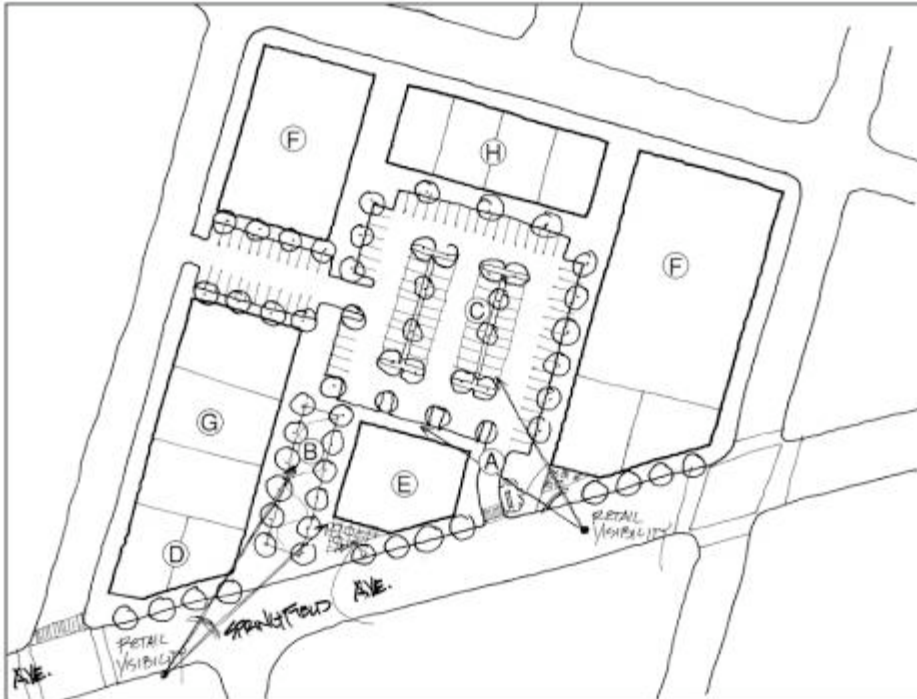
- Buildings must be set at “zero lot line” setbacks for a minimum of 70% of the linear frontage along Springfield Avenue.
- All buildings fronting on Springfield Avenue must have direct access from the Springfield Avenue sidewalk.
- Breaks in the street wall shall be a maximum of 60’ wide and shall only be constructed to accommodate vehicular access to rear parking, pedestrian access to interior retail and parking, and to construct ample, well lit public plazas.
- Public plazas and spaces should be incorporated whenever possible
- Loading, refuse and other functional areas shall be located as to not impact adjacent residential neighborhoods and, whenever possible, should be located to the rear of buildings and in such a location so as to minimize disruption to traffic on Springfield Avenue during access.
- Street fronting retail with parking behind should have dual “storefronts” located on the street and the parking area.
- Ample sidewalks with a minimum of 15’ clearance are to be provided along interior retail spaces fronting the parking lot. This space should be treated as a “street” and should adhere to the streetscape guidelines.
- Shop fronts must line the sidewalk and face the shops on the opposite side of Springfield Avenue.
- The “rear” of commercial centers, whether they are new buildings or parking lots, must be planned to be compatible with the adjoining residential neighborhoods.



Plan



Plan



Legend

- A. Primary access – Greater than 60'-0"
- B. Public plaza – Greater than 60'-0"
- C. Green infrastructure – Bioswale, pervious pavers, etc.
- D. Small-scale cafes and shops face Springfield Avenue
- E. Freestanding retail pad
- F. Anchor stores
- G. Mid-size stores
- H. Neighborhood retail adjacent to residential

Example of possible development pattern within the movie theater site

4.5.2 Building Form



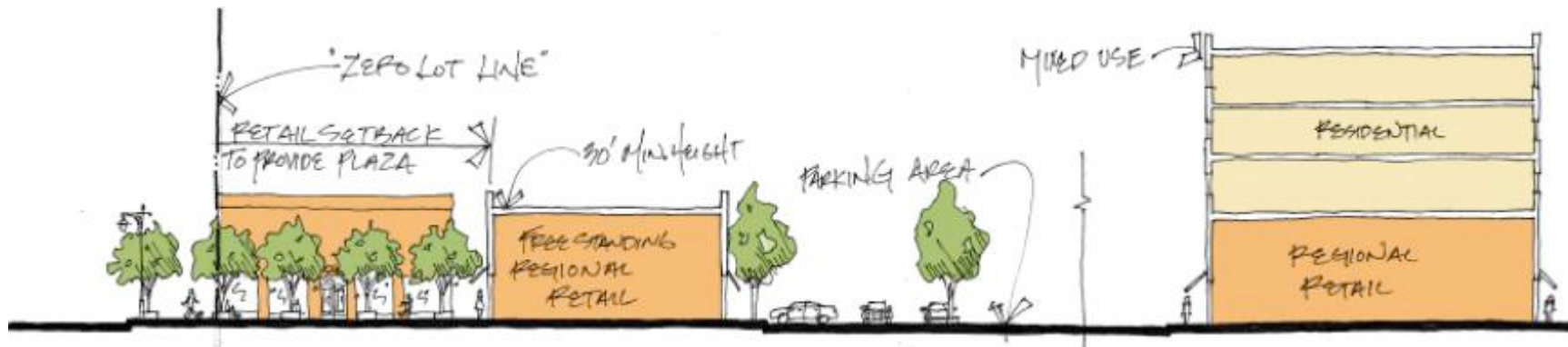
Large format retail with stepback

Summary

These guidelines intend to allow for variation in height and form in order to allow flexibility in the current retail market. Buildings are encouraged to be mixed-use, but freestanding retail is permitted.

Standards and Guidelines

- Buildings shall be a minimum of 1 story or 20' in height, but can be up to 5-stories or 60' at the property line, and up to 7-stories within the sky exposure plane.
- Balconies, parapet walls and other architectural elements are permitted to pierce the sky exposure plane.
- Architectural elements such as cornices and balconies should be located at varying heights between 15' and 40' in order to reduce the perceived scale of the building and mimic the historic varied building heights along Springfield Avenue.



Typical section

4.5.3-4 Building Face and Site Landscaping



Large format retail screened with trees along perimeter of property

Summary

The Building Face and Site Landscaping requirements for this section shall be identical to those in the MUCR District, detailed in Sections 4.4.3 – 4, except for the additional requirements listed below.

Guidelines and Standards

- Any large retail structure, side or rear facades must be well-screened from the Residential District at the edges of the parcel.
- Large blank walls, truck docks, and mechanical equipment are not permitted to face the nearby residences. The blank walls and truck docks must be well set back from the homes that exist (or will exist) on the streets at the back of the structure and must be screened by landscaping or decorative walls. Large walls should include some pattern and color in the masonry and other building details.
- No blank walls on the anchor buildings can face Springfield Avenue. All truck docks must be screened by tall masonry walls and tall landscaping. The large blank walls of the backs of the buildings must be screened by tall trees and low ground covers; those same

4.6 Surface Parking Lots and Garages

4.6.1 Layout and Location



Landscaped parking lot divided into sections



Parking lot with extensive shade tree coverage

Summary

This section addresses the standards and guidelines for all surface parking lots throughout the redevelopment area to ensure that proper care is taken when locating and laying out parking lots.

Standards and Guidelines

- All surface parking lots shall be behind buildings and shall not occupy primary street frontage or corners whenever possible.
- No parking shall be permitted between the front façade of the building and the street edge.
- Larger parking areas should be visually divided into smaller parking courts with the use of landscaping improvements.
- No more than 20 consecutive parking spaces shall be permitted in any row without being broken down by planting areas, aisle and shade trees.
- If a project is to be constructed in phases, only the parking associated with that phase shall be constructed. Open areas for future phases shall be appropriately landscaped.
- Access between adjacent parking areas shall be provided whenever possible.
- Minimum driveway widths and radii shall be used according to use and demand. Avoid over-sizing.
- Provide continuous vehicular circulation throughout the site. Dead-end parking shall not be permitted unless unavoidable due to site restrictions. If dead-end parking is unavoidable, the row shall be no greater than 8 cars deep.
- Parking lots must have safe and well-lit walkways to connect sidewalks and storefronts.
- Parking lot driveways that pass the fronts of the major retail businesses should be consistent with design guidelines of the City street system, with sidewalks that pass open storefronts and provide connections to neighborhoods and Springfield Avenue.
- Anchor store parking lots must be available for use by existing retail buildings across Springfield Avenue. It must be screened from the neighborhood by either double rows of trees, or preferably, a line of new homes.
- All parking garages must be enclosed for all facades that are visible from the street.
- Solid glazing is required at all street front facades.
- Open fenestration for parking garages may begin no less than three feet in height from the ground.



Enclosed parking garage façade

- Glare from internal lighting shall not spill out onto the street.
- Retail and commercial uses should wrap around parking at street level facing the sidewalk.

4.6.2 Landscaping and Greening



Shade trees planted throughout parking lot



Parking lot bioswale

Summary

This section addresses the standards and guidelines set forth for all surface parking lots throughout the redevelopment area to ensure proper care is taken to provide vegetation, shade and environmentally sustainable technologies.

Standards and Guidelines

- A maintenance and upkeep program for all elements of surface parking lots, including surfaces, landscaping, amenities, and other elements, shall be developed and provided to the City for review.
- Shade trees shall be provided to ensure that 50% of all parking surfaces will be shaded upon trees reaching maturity.
- A minimum of 1 tree shall be for every 5 parking spaces.
- The centerline of trees shall be located a minimum of 4' from face of curb.
- All plant material, excluding sod and ground cover, shall be located a minimum of 2' from face of curb.
- Trees shall be planted in ample tree pits adjacent to the curb with a minimum of 4' width and 24 square foot area. Whenever possible, large continuous planting "trenches" shall be provided and backfilled with structural soil in order to provide contiguous space for root growth below pavers. Removable pavers can be used to fill voids in the planting strip allowing enough space for the tree trunk to expand. A 3' minimum depth of appropriate planting medium shall be provided.
- All shade trees shall be a minimum of 3" caliper at time of planting.
- All shade trees shall be branched at a minimum of 7' height. Trees with upright branching habits shall be used in areas heavily traveled by trucks (i.e. service roads, main entrances, etc.) so as to avoid contact with vehicles.
- Expand rooting area of trees whenever possible through the use of cells or structural soils.
- The perimeter of all parking areas adjacent to streets shall be appropriately screened. A



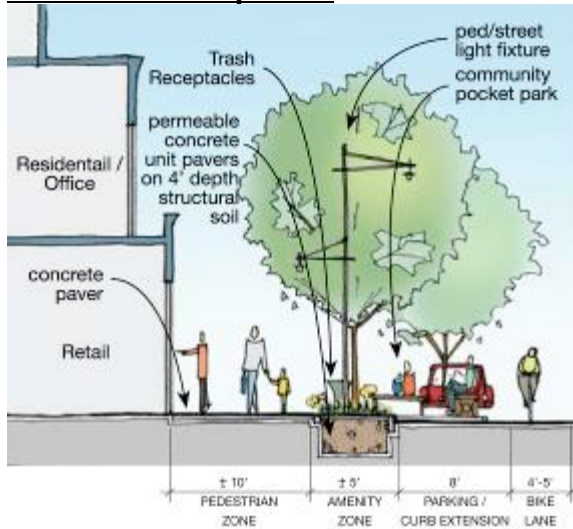
Parking lot utilizing pervious paving materials

combination of landscape berms and evergreen shrubs shall be used so that a continuous screen 3' in height is created.

- All plant material selected shall be suitable to harsh urban environments and parking lot conditions.
- Species that are drought tolerant, salt tolerant and resistant to stress caused by compacted soils shall be used whenever possible.
- Suitable native plant species shall be used whenever possible
- A variety of plant species and trees shall be used to avoid monoculture plantings.
- Use a variety of deciduous and coniferous shrubs that provide year-round interest and color.
- Bioswales shall be created between parking stalls to encourage infiltration
- Pervious pavers and surfaces shall be used wherever possible.
- All parking lots should include space for shade trees, especially along their edges, to create a sense of a closed open space. Trees at the edges should be no more than 50' apart.
- Parking lots that open onto residential streets should include, at a minimum, a double row of shade trees along the street edge. If possible, a row of residential development that backs onto the commercial property and faces the neighborhood street should be considered.
- Transparent decorative fencing may be incorporated into perimeter landscaped parking areas, but may not exceed 3' in height.

4.7 General Requirements for All Streets

4.7.1 Public Spaces



Cross-section of a public space



Paver edge to define "amenity zone"

Summary

This section addresses the standards and guidelines set forth for streets throughout the redevelopment area in order to ensure a safe, attractive, unified and sustainable environment for all users. These guidelines are consistent with creating walkable commercial/neighborhood centers with residential/mixed-use infill between them and aim to create a pedestrian oriented Springfield Avenue. The shopping experience in the neighborhood centers should be based on plans and designs that make walking from store to store comfortable and convenient. These standards shall be applied to all streets regardless of location or ownership.

General Standards and Guidelines

Lighting

- Provide pedestrian scale light fixtures.
- Pedestrian scale lights should not exceed 14' in height.
- Dual height poles that accommodate both pedestrian and vehicular scale light poles are encouraged to reduce clutter.
- Lighting standards shall be in compliance with City standards.

Paving

- Provide permeable surfaces for driveways whenever possible (i.e. permeable paver systems).
- All pedestrian crossings shall utilize materials and colors to readily distinguish vehicular from pedestrian travel ways. Appropriate locations for bump-outs along Springfield Avenue where buses may pull over for passengers to safely embark and disembark should be identified and created as development progresses.
- All new construction on Springfield Avenue with more than 150 feet of width will be required to add 10 feet to the public right-of-way in order to accommodate a parking lane and/or a BRT bus stop.
- All crosswalks shall be colored and textured using either concrete unit pavers or stamped resin, as is being done elsewhere in the City.



Signs, bicycle rack & lighting in “amenity zone”



Brick pavers with concrete banding and textured crosswalk

- Raised or tabled crosswalks shall be implemented at major pedestrian crossings, particularly crossings that connect to park entrances.
- Sidewalks are to be provided on both sides of all streets.
- Ample, unobstructed sidewalks a minimum of 7' in width shall be provided. This width should be clear of trash receptacles, street furniture, lighting, landscaping or other obstructions.
- An additional 4' “amenity zone” should be identified in the MUCR District, adjacent to the curb and outside of the sidewalk area, for the location of trees, lighting and furnishings.
- Pavers above structural soil should be “pervious pavers” if possible.
- Sidewalks should incorporate “decorative” pavement materials such as concrete unit pavers, brick or stamped concrete.
- Maintenance, wear, durability and stain resistance should all be considered when selecting a sidewalk material in a commercial zone.

Landscaping

- Maximize landscaped areas both within and outside of the public right-of-way while considering safety, sight triangles and maintenance
- Tree species shall alternate throughout the length of the street to avoid monoculture. No more than 5 of the same species shall be planted in any consecutive row.
- Buffers between properties and driveways shall be appropriately landscaped to maximize vegetated areas and buffer properties.
- Street trees should have high, vertical branching habits as to not obstruct views of retail.
- Street trees should be spaced at 30' on center wherever feasible based on overhead and underground conditions. In all cases, street trees shall be spaced 25' - 40' maximum on-center on all streets, depending on ultimate size of tree selected. Spacing shall be done in such a way that the ultimate size of the tree will provide a consistent canopy and shading of the sidewalk areas.
- Trees shall be planted in ample tree pits adjacent to the curb with a minimum of 4' width and 24 square foot area. Whenever possible, large continuous planting “trenches” shall be provided and backfilled with structural soil in order to provide contiguous space for root growth below pavers. Removable pavers can be used to fill voids in the planting strip allowing enough space for the tree trunk to expand.
- Properly amended soil shall be used per industry standards to a depth of 3' within the entire planting pit to ensure proper long term success of street trees.
- All street trees shall be branched at 6.5' height minimum to avoid having branches



Amenity zone with street furniture

- obstruct the pedestrian circulation and conflict with vehicles.
- Curb extensions shall be provided at corners and mid-block crosswalks whenever road width and turning radii permit.
- Car/pedestrian conflicts should be reduced by minimizing curb cuts and consolidating driveways.
- Public rights-of-way shall be designed to meet all local, state and federal standards.

Street Furnishings

- Street furnishings including benches, bicycle racks and trash receptacles should be provided in groupings throughout the streetscape.
- Bicycle racks should be strategically located near primary entrances, corners or gathering spaces and placed within the 4' "amenity zone" along the curb.
- Benches shall be located throughout all neighborhoods. More benches shall be located in areas of higher use or density.
- Trash receptacles shall be located throughout commercial areas, parks and other gathering places.
- All amenities including, but not limited to, lights, signs and sign poles, and trash receptacles shall be selected to be architecturally appropriate for the neighborhood.
- The level of all facilities provided shall be in accordance with NJDOT standards.

Signage

- Whenever possible, signs shall be coordinated and clustered on single poles to reduce clutter and minimize obstructions.
- Pedestrian signage shall be provided on all major pedestrian circulation streets.
- See Section 4.8 for further detail.

4.7.2 Pedestrian & Bicycle Access



Bicycle racks adjacent to retail shopping

Summary

The section addresses the standards and guidelines set forth for all sidewalks and streets throughout the redevelopment area to ensure safe and appropriate facilities are provided for pedestrians and cyclists.

Standards and Guidelines

- Unobstructed pedestrian and vehicular fields of vision should be provided at all crossing areas, vehicular routes and pedestrian paths.
- Bicycle racks and parking areas shall be provided near main entrances to all retail buildings.
- All pedestrian routes shall be a minimum of 5' wide and unobstructed, unless prohibited by site conditions.
- The width, quantity and orientation of pedestrian routes shall be directly related to the anticipated volume, type and flow of pedestrian traffic. For example, if shopping carts or strollers are anticipated, wider sidewalks should be provided.
- Raised or tabled crosswalks can be installed at all major pedestrian crossings to act as a traffic calming device.
- All pedestrian crossings shall be well lit and clearly marked using signs and paving materials. Care should be taken to ensure clear and unobstructed visibility in these areas.

4.8 Signs and Awnings



Storefront with building mounted sign



Storefront with building mounted sign

Summary

This section addresses the standards and guidelines for all new signage within the redevelopment area and shall apply to all five new Districts.

Signs and Awnings Guidelines

All commercial buildings are permitted to have up to 3 types of signs, as listed below, at the discretion of the planning board.

Awning Signs

An awning sign is a building-mounted sign that provides additional functionality as shelter. Awning signs are typically mounted over entrances and storefronts. Requirements:

- Signs are limited to 50% of each awning or 10 square feet, whichever is less.

Building Mounted Signs

A building mounted sign is a sign that is applied or attached to a building. Also known as a fascia sign or a wall sign, these signs are typically located in a horizontal band across a building entrance.

- Wall mounted identification signage must be designed to fit within the architecture of the building and may not employ illuminated sign boxes with plastic faces.
- Façade signs shall be limited to 5% of the facade area or 20 square feet per storefront, whichever is greater.
- Individual letter signs are permitted and can use wood, metal or acrylic.

Projecting Signs

A projecting sign is mounted on a building with the faces of the sign perpendicular to the building facade. Examples of projecting signs include paddle signs and banners.

- Projecting signs are limited to 12 square feet and must be made from solid materials including carved wood or engraved metal.

Window Signs

A window sign is a sign that is displayed on or behind the storefront glazing. The signage graphics typically adhere directly to the glass.

- Signs are limited to 20% of each storefront window or 10 square feet, whichever is less.
- Flashing and blinking lights are prohibited.

Miscellaneous Signs



Storefront with projecting sign

- A Sidewalk Sign is a moveable sign not secured or attached to the ground or surface upon which it is located, but supported by its own frame and most often forming the cross-sectional shape of an “A.” These signs are typically utilized by eating and drinking establishments to display specials during business hours.
- A Monument Plaque is a commemorative sign that is intended to remind the community of a person, a building or an event of significant importance. Monument plaques are typically made of engraved stone or cast metal.
- Neon Signs may be permitted at the Planning Board’s discretion, but must adhere to regulations set forth in Window Signs section.
- Additional creative sign solutions are welcome and may be permitted at the Planning Board’s discretion.

4.9 Parks and Open Space

To enhance the value and increase the use of West Side Park, the West Side Park Conservancy has worked with community groups to develop a plan for improvements to its interior and to the area immediately surrounding it. In order to advance and complement the Conservancy's efforts and designs, the following standards should be adhered to:

1. Light fixtures

- Pedestrian scale - County standard park light fixture shall be used along the park side of the parking lot
- Vehicular scale -- A double pendant fixture shall be used to help light the road surface along the curb edge so pedestrian and roadway lighting can be accomplished with pairs down the street.

2. Benches - County standard benches shall be used, per West Side Park Master Plan.

3. Trash receptacles - County standard trash receptacles shall be used, per West Side Park Master Plan.

4. Furniture color - Black, semi-gloss colors shall be used

5. Trees and Tree spacing - The tree spacing that already exists along the edge of West Side Park shall be continued, and London Plane trees shall be used around the park perimeter. A different tree species may be used on the perpendicular streets, upon specific approval. A continuous soil panel should be used for root growth support.

6. Sidewalks - A broom finish or exposed aggregate concrete paving should be used.

4.10 Provisions Related to Off-Site Improvements

The designated redeveloper or another party responsible for the development of a property in the redevelopment area shall be responsible for their fair share of any installation or upgrade of infrastructure related to their project, whether on-site or off-site. Infrastructure items include, but are not limited to, gas, electric, water, sanitary and storm sewers, traffic control devices, telecommunications, streets, curbs, sidewalks, street lighting and street trees. The extent of the redeveloper's responsibility will be outlined in the redeveloper's agreement with the City. Off-site responsibility for properties not covered under the redeveloper's agreement will be determined during the permit and/or site plan review phases.

All infrastructure improvements shall comply with applicable local, state and federal codes, including the Americans with Disabilities Act. All utilities shall be placed underground.

4.11 Provisions Related to State and Federal Regulations

Certain redevelopment activities proposed in this plan may be subject to state and federal standards, regulations and permit requirements, including but not limited to, post-remediation requirements. The redeveloper is responsible for ensuring compliance with all applicable standards and obtaining necessary state and federal permits.

Section 5. Relationship to Zoning and Land Use Ordinance

In order to achieve the purposes of the Plan, the development and use of land within the Study Area shall be in accordance with the land use provisions and standards set forth in this Plan and shall supersede in the event of any conflict with existing or future zoning ordinance provisions. In the event that a particular land use or site standard is not covered in this redevelopment plan, compliance with the Newark Zoning Ordinance or other applicable Newark code or ordinance will be required.

Zoning Map Revisions

The Zoning Map of the City of Newark is hereby amended in accordance with the Future Land Use map to indicate the boundaries of the Redevelopment Plan Area and to identify the district as the KBS Redevelopment Plan Area. The Zoning Map is amended to show the boundaries of the Redevelopment Plan Area. Land Uses and development requirements for the land use categories are as provided in this Plan.

Section 6. Relationship to Other Plans

As required by the New Jersey Local Redevelopment and Housing Law, this section describes the consistency and relationship between the KBS Redevelopment Plan and: (a) the Master Plan of contiguous municipalities (Irvington) and other Newark City plans, (b) the Essex County Master Plan, and (c) the New Jersey State Development and Redevelopment Plan.

6.1 Plans of Adjacent Municipalities

The KBS Redevelopment Plan Area is located in the western portion of Newark. This area abuts the East Ward Redevelopment Area in Irvington, an area that has been undergoing similar redevelopment activities.

The land uses categories on the Irvington and Newark sides of the border are consistent and potentially compatible. The KBS Plan will not materially change many of the existing land use conditions other than to facilitate a mix of pre-existing uses in certain areas. In addition, this plan is consistent with and implements the recommendations in the Springfield Bergen Redevelopment Plan and the Springfield Avenue Corridor Smart Growth Handbook, which was prepared for the Newark Economic Development Corporation and the Irvington Township Urban Enterprise Zone Program and incorporated into the newly adopted Newark and Irvington Master Plans.

It is anticipated that the redevelopment occurring in both municipalities will complement and strengthen each other's respective effort, resulting in vibrant and revitalized areas.

6.2 Essex County Master Plan

The most current version of the Essex County Master Plan is the Land Use and Housing Plan Element (August, 1980). A review of this document found that the following excerpts from the Goals and Objectives section of Essex County's 1980 Plan are consistent with the Kent/Brenner/Springfield Plan:

- "To promote the restoration, preservation, and development of existing commercial areas. Such action will generate additional business and remove the blighting influence of commercial uses on nearby residential neighborhoods.
- "To encourage the development of multi-purpose cluster zones with diversified retail facilities and adequate off-street parking. This land use pattern would discourage the further fragmented development of commercial strips along major arteries which impede the efficient flow of traffic."

Included in the recommendations of the Plan are the following:

- That municipalities grant tax abatements for qualified affordable units to encourage development.
- Protect and restore established neighborhoods by encouraging housing rehabilitation as an alternative to new development.

While the current version of the Essex County Master Plan is more than 20 years old, it remains substantially consistent with the City's ongoing planning efforts to revitalize, reinforce, and support its neighborhoods and commercial areas.

6.3 New Jersey State Development and Redevelopment Plan

On March 1, 2001, the State Planning Commission adopted the current New Jersey State Development and Redevelopment Plan (SDRP). The SDRP is a guide for investing and spending state dollars in a manner that is consistent with the Plan's goals, yet is voluntary for municipalities to follow.

The 2001 Plan identifies all of the City of Newark as part of Metropolitan Planning Area (PA-1). The KBS Redevelopment Plan is compatible with the State Plan's intentions for PA-1 to provide for much of the state's future redevelopment by pursuing the following principles:

- Revitalize cities and towns
- Promote growth in compact forms
- Stabilize older suburbs
- Redesign areas of sprawl
- Protect the character of existing stable communities

The KBS redevelopment plan also furthers the following State Plan policy objectives:

- Land Use - Ensure efficient and beneficial utilization of scarce land to strengthen its existing diversified and compact nature.
- Housing - Provide a full range of housing choices through redevelopment, new construction, rehabilitation, adaptive reuse and the introduction of new housing into appropriate nonresidential settings.
- Economic Development - Promote economic development by encouraging strategic land assembly, site preparation and infill development, public/private partnerships and infrastructure improvements.
- Natural Resource Conservation - Reclaim environmentally damaged sites and mitigate future negative impacts. Use open space to reinforce neighborhood and community identity.
- Redevelopment - Encourage redevelopment at intensities sufficient to support transit, a broad range of uses and efficient use of infrastructure. Promote design that enhances public safety, encourages pedestrian activity and reduces dependency on the automobile.
- Public Facilities and Services - Complete, repair or replace existing infrastructure systems to eliminate deficiencies and provide capacity for sustainable development and redevelopment.

The State Plan designates Newark as an “Urban Center,” a place offering the most diverse mix of industry, commerce, services, residences and cultural facilities. The Kent/Brenner/Springfield Plan is consistent with this designation, and will create a vibrant, fully productive, mixed-use oriented neighborhood that meets the goals of the SDRP.

Section 7. Amendments to and Duration of the Redevelopment Plan

7.1 Amendments to the Redevelopment Plan

This plan may be amended from time to time in accordance with the procedures of the Local Redevelopment and Housing Law.

7.2 Certificates of Completion and Compliance

Upon the inspection and verification by the City of Newark's redevelopment entity that the redevelopment of a parcel subject to a redeveloper agreement has been completed, a Certificate of Completion and Compliance shall be issued to the redeveloper and such parcel shall be deemed no longer in need of redevelopment.

Section 8. Definitions and Waivers

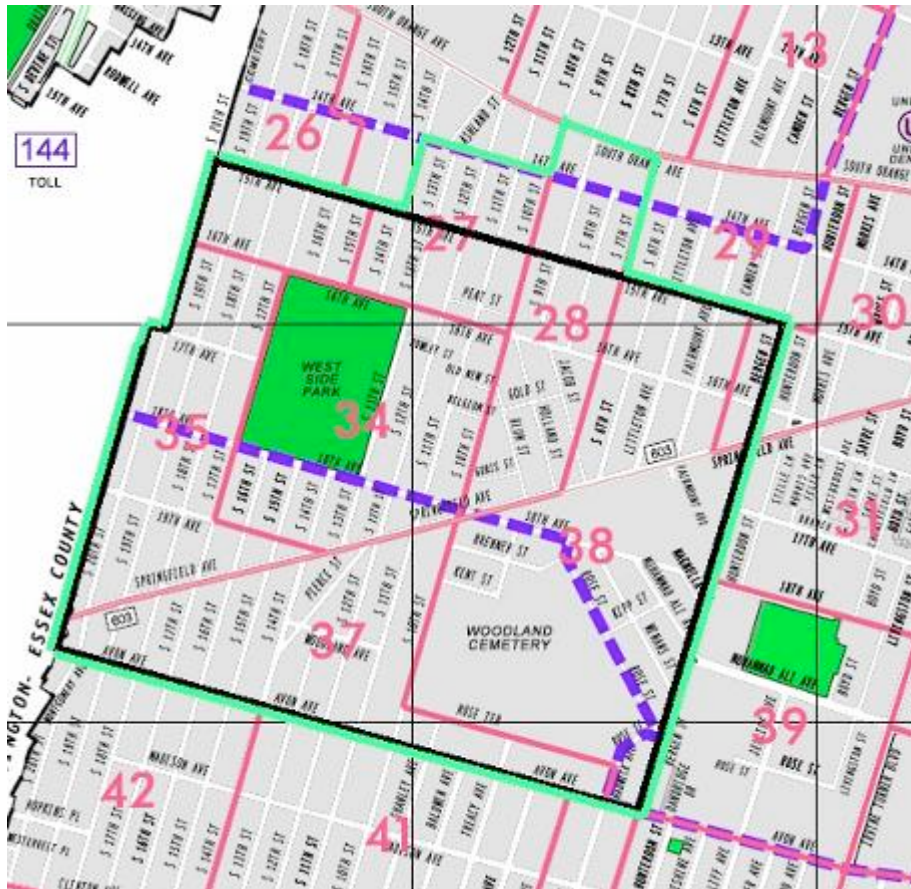
Definitions

All terms used herein shall have the same meaning as defined in the Newark Zoning Ordinance unless otherwise specified in this redevelopment plan.

Waivers

The Central Planning Board may grant waivers from the regulations contained within this Plan, where, by reason of exceptional narrowness, shallowness or shape of a specific piece of property, or by reason of exceptional topographic conditions, pre-existing structures or physical features uniquely affecting a specific piece of property, the strict application of any area, yard, bulk or design objective or regulation adopted pursuant to this Plan, would result in peculiar and exceptional practical difficulties to, or exceptional and undue hardship upon, the developer of such property. The Central Planning Board may also grant a waiver from the regulations contained within this Plan related to a specific piece of property where the purposes of this Plan would be advanced by such waiver from the strict application of the requirements of this Plan; and the benefits of granting the waiver would outweigh any detriments. The Central Planning Board may grant exceptions or waivers from design standards, from the requirements for site plan or subdivision approval as may be reasonable and within the general purpose and intent of the provisions for site plan review and/or subdivision approval within this Plan, if the literal enforcement of one or more provisions of the plan is impracticable or would exact undue hardship because of peculiar conditions pertaining to the site. No waivers may be granted under the terms of this section unless such waivers can be granted without resulting in substantial detriment to the public good and will not substantially impair the intent and purpose of this Plan. An application requesting a waiver from the requirements of this Plan shall provide public notice of such application in accordance with the public notice requirements set forth in NJSA 40:55D-12.a. & b.

Appendix A – Census Tract Map



- Census Data Boundary
- KBS Redevelopment Plan Boundary
- Census Tract Boundaries